

IRVINGTON CONCEPT PLAN

REVISED DRAFT



CITY OF FREMONT

APRIL 2004



PREPARED BY:
DESIGN, COMMUNITY & ENVIRONMENT

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TABLE OF CONTENTS

Introduction	I
A. Planning Area	I
<i>Figure I-1. Fremont's Location in the Bay Area</i>	<i>I</i>
<i>Figure I-2. Irvington Vicinity</i>	<i>2</i>
<i>Figure I-3. Irvington Concept Plan Area</i>	<i>3</i>
B. Planning Time Frame	4
C. Planning Process	4
D. Concept Plan Overview	6
I Existing Conditions	7
A. Public Policy	7
<i>Figure I-1. General Plan Designations</i>	<i>8</i>
<i>Figure I-2. Zoning</i>	<i>9</i>
B. Existing Land Uses	10
<i>Figure I-3. Existing Land Uses</i>	<i>11</i>
C. Urban Design	12
<i>Figure I-4. Existing Street Network at Five Corners</i>	<i>12</i>
D. Seismic Considerations	14
<i>Figure I-5. Hayward Fault</i>	<i>14</i>
E. Market Conditions	15
F. Planned Public Improvements	16
<i>Figure I-6. Planned Public Improvements and Opportunity Sites</i>	<i>17</i>
G. Proposed Public Opportunity Sites	18
H. Existing Utilities and Easements	19

2 Plan Vision and Goals	21
A. The Vision for Irvington	21
<i>Figure 2-1. Perspective Illustration of Five Corners from Bay Street</i>	21
B. Concept Plan Goals	22
3 Development Concepts	23
<i>Figure 3-1. Overall Development Concept</i>	23
<i>Figure 3-2. Focus Areas Diagram</i>	24
A. Bay Street/Monument Center	25
<i>Figure 3-3. Bay Street/Monument Center Concept</i>	25
<i>Figure 3-4. Concept Plan vision for Bay Street</i>	26
<i>Figure 3-5</i>	
A. Bay Street Schematic Design Concept (West)	27
B. Bay Street Schematic Design Concept (East)	28
<i>Figure 3-6 Monument Center</i>	
A. Alternative 1	29
B. Alternative 2	29
C. Alternative 3	29
<i>Figure 3-7. Bay Street Post Office Parking Alternative</i>	30
B. Washington/Main Street Triangle	31
<i>Figure 3-8. Washington/Main Street Triangle Development Concept</i>	31
<i>Figure 3-9. A view across Five Corners from Bay Street</i>	32
<i>Figure 3-10. Concept Plan Vision for Five Corners</i>	33
C. Main Street	34
<i>Figure 3-11. Main Street Development Concept</i>	35
<i>Figure 3-12. Main Street Section</i>	35
<i>Figure 3-13. Main Street Intersection at Roberts Avenue</i>	36
<i>Figure 3-14. A view on Main Street</i>	37
<i>Figure 3-15. A view of the Main Street Terminus</i>	38
<i>Figure 3-16. Main Street Terminus at Railroad Tracks</i>	39
<i>Figure 3-17 Washington Boulevard Bridge and Parking Garage</i>	41
D. BART Station Site	42
<i>Figure 3-18. Irvington BART Station Development Concept</i>	42

E. Railroad Parcels	44
<i>Figure 3-19. Railroad Parcels Development Concept.....</i>	<i>45</i>
F. Grimmer South	46
<i>Figure 3-20 Grimmer South Development Concept</i>	<i>46</i>
G. Fremont Boulevard	47
<i>Figure 3-21. Fremont Boulevard Development Concept.....</i>	<i>47</i>
H. Creeks	48
<i>Figure 3-22. Laguna Creek in Irvington</i>	<i>48</i>
I. Concept Plan Illustrative	48
<i>Figure 3-23. Development Concept Preferred Illustrative</i>	<i>49</i>

4 Design Guidelines	51
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A. City of Fremont Project Review Process	51
B. Design Guideline Categories	52
C. Definitions	52
<i>Figure 4-1. Street Categories for Design Guidelines</i>	<i>53</i>
D. Pedestrian-Oriented Streets	54
E. Major Streets	73
F. Guidelines for Primary Historic Resource Structures	82

5 Plan Implementation	85
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A. Implementation Measures	85
<i>Figure 5-1. Catalyst Projects</i>	<i>87</i>
B. Phasing of Implementation Measures	90
<i>Table 5-1. Implementation Matrix</i>	<i>93</i>

Appendix A Demographics and Market Analysis A-I

A. Demographic/Economic Trends A-I

B. Real Estate Market Conditions A-I I

Appendix B Sales Leakage Analysis B-I

A. Analytical Methodology B-I

B. Retail Leakage and Supportable Square Feet B-4

C. Conclusions B-6

D. Summary B-8

Appendix C Traffic Information C-I

A. Bay Street/Monument Center C-I

B. Washington/Main Street Triangle C-2

C. Main Street C-2

D. BART Station Site C-3

E. Railroad Parcels C-4

F. Grimmer South C-4

G. Fremont Boulevard C-5

H. Creek Areas C-5

INTRODUCTION

The Irvington Concept Plan identifies the vision for development of Fremont's Irvington District over the next 20 years, and provides guidelines for future development.

This document outlines a long-range plan that contains the vision and goals for Irvington and provides steps that should be taken in order to accomplish those goals. Conceptual designs and illustrative site plans provide examples of how specific areas of Irvington may be developed. Design guidelines and implementation strategies provide further details for development in Irvington. The drawings are illustrative only and do not reflect specific design requirements for the City, property owners or developers.

In the interim, until land use and zoning changes are implemented by the City, current land use and zoning rules will apply. If, however, a property owner or developer is interested in implementing concepts and visions outlined in this plan prior to the City-initiated changes, they may initiate changes by filing the necessary applications with the City.

A. Planning Area

Fremont is located in Alameda County on the southeast side of San Francisco Bay. In the nine county Bay Area region, Fremont is the fourth largest city by population, with over 208,000 residents, and the largest city by geographic area, with a 92-square mile area. The city is located

between the Bay and the hills that run the length of the East Bay.

Fremont is located in the northeast end of Silicon Valley, which is home to specialized high tech and bio-tech industries and a competitive workforce. Over the past 20 years, Fremont has been influenced by the Silicon Valley's dramatic economic growth and increased housing demand.

Irvington is one of five historic towns that joined to become the City of Fremont. It was originally a small town that developed at the junction between two major roads and the rail line that linked Oakland and San Jose. As shown in Figure I-2, today's Irvington District is bordered by Interstate 880 to the west and on the other sides by the Central Business District, Mission San Jose and the Industrial Area.

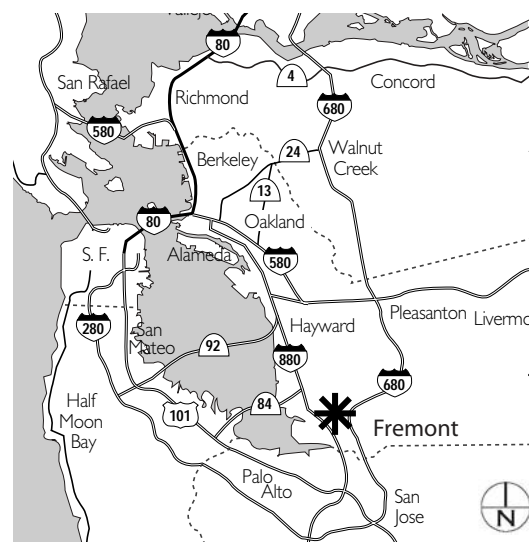


Figure I-1. Fremont's Location in the Bay Area

INTRODUCTION

This Concept Plan addresses the commercial core area of Irvington which, as shown in Figure I-3, is centered on the Five Corners intersection of Fremont Boulevard, Union Street, Washington Boulevard and Bay Street.

The Concept Plan Area accommodates a number of land uses that include retail stores, offices, restaurants, specialty grocery stores, medical offices, schools, residences and some civic uses. Retail buildings range in size from a regional retailer, to smaller neighborhood-serving businesses. Most buildings in the district are one or two stories tall, with some three-story multi-family residential buildings. Generally, housing

within the Concept Plan Area consists of older single-family residences on small lots. Multi-family garden apartment complexes and condominium developments are located on several larger half block areas or aggregated parcels. Arterial streets such as Fremont Boulevard and Washington Boulevard run through the area and meet at Five Corners.

A majority of the Concept Plan Area lies within the boundaries of the Irvington Redevelopment Area. In 1998, the Redevelopment Area was increased by an additional 323 acres for a total area of 473 acres. Agency funding has implemented a number of streetscape and public improvements in Irvington in recent years.

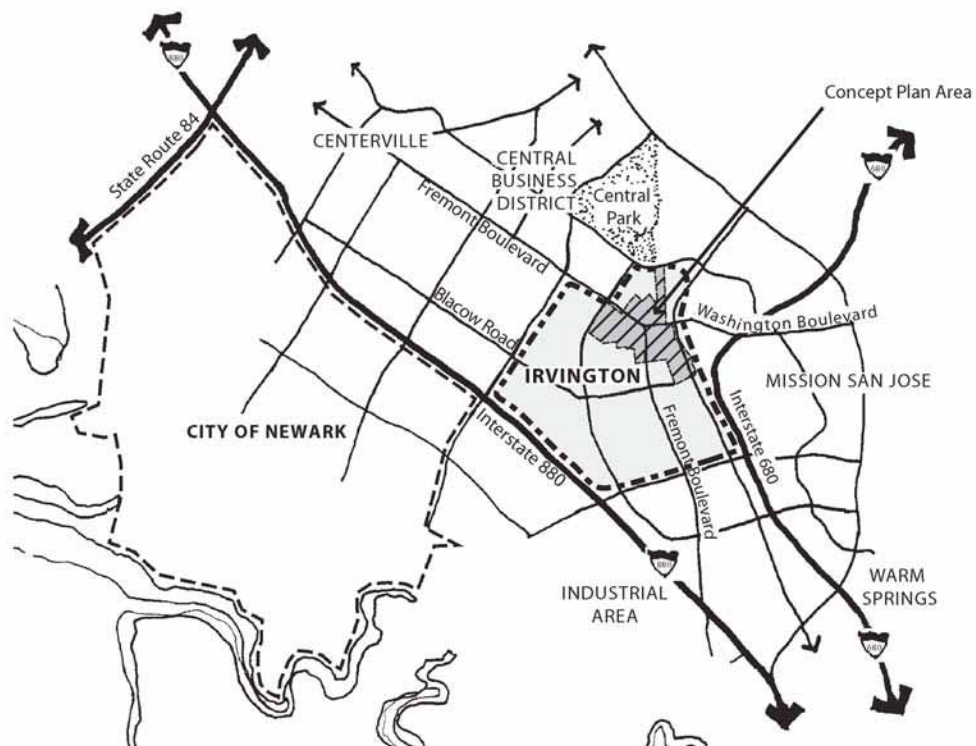


Figure I-2. Irvington Vicinity

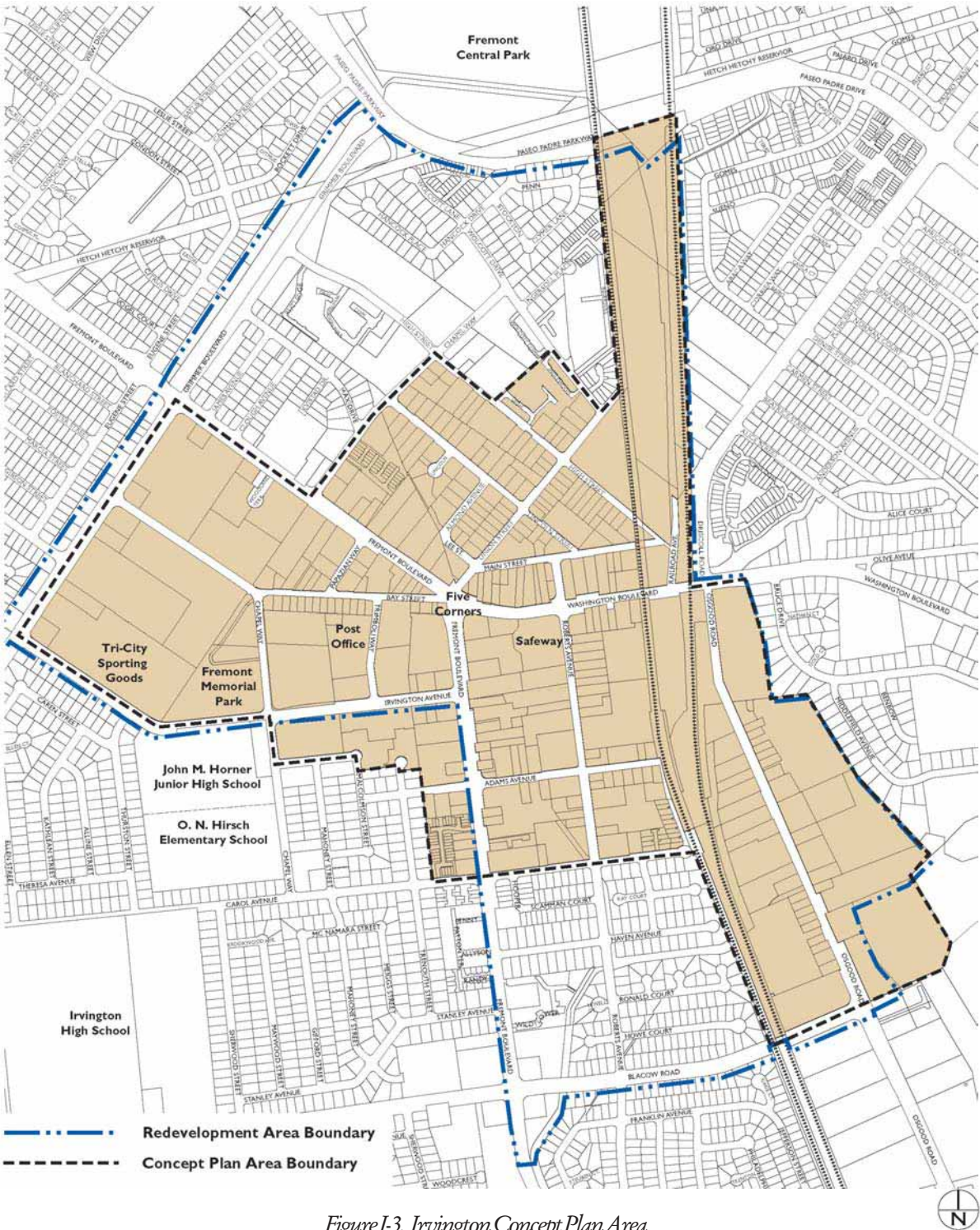


Figure I-3. Irvington Concept Plan Area

INTRODUCTION

B. Planning Time Frame

Within the 20-year planning timeframe, construction of the Washington and Paseo Padre Grade Separation Projects and the accompanying track realignment, the potential construction of an Irvington BART station, and continuing demand for various types of housing products will create a number of significant redevelopment and revitalization opportunities in Irvington.

C. Planning Process

The planning process for the Concept Plan has been a collaborative effort between City staff, Irvington community members, local business owners and a consultant team led by Design, Community & Environment (DC&E). A central goal of the planning process was to include substantial community participation. Input for the Concept Plan was given through community participation workshops, stakeholder interviews, and a developer and retailer panel discussion, as well as input from the Irvington Business Association (IBA), an ad hoc committee, and staff members representing a number of City departments.

The community participation and review process included the following meetings and workshops:

- ◆ **Existing Conditions and Visioning Workshop** (September 19, 2001).

- ◆ **Developer and Retailer Panel Discussion** (November 1, 2001).
- ◆ **Alternatives Creation Workshop** (November 15, 2001).
- ◆ **City Council Update** (January 29, 2002).
- ◆ **Alternatives Review Workshop** (January 31, 2002).
- ◆ **Draft Illustrative Review Meeting** (May 15, 2002).
- ◆ **Monument Center Redevelopment Workshop** (April 24, 2002).
- ◆ **Bay Street Stakeholders Meeting** (July 23, 2002).
- ◆ **Community Review** of First Draft Plan (October 23, 2002).
- ◆ **Hearings** on the First Draft Plan before the Planning Commission and City Council (February-July 2003).
- ◆ **Workshops** with Irvington stakeholders to create this Revised Draft Plan (November 2003 - January 2004).

This Revised Draft Plan will be reviewed at additional workshops and hearings with Irvington stakeholders, the Historic Architectural Review Board, the Planning Commission and the City Council.

D. Concept Plan Overview

This Irvington Concept Plan outlines the goals and vision to guide future development within the Irvington District. The Plan presents the overall concept for the district and describes concepts for eight key focus areas. Design guidelines are recommended that would help support the development of the vision and goals. These components are described in the following chapters:

- ◆ **Chapter 1, Existing Conditions:** Summary of the background research on existing urban design, market, traffic and circulation conditions in Irvington.
- ◆ **Chapter 2, Plan Vision and Goals:** A listing of the overall vision and goals of the Concept Plan.
- ◆ **Chapter 3, Development Concepts:** Descriptions of the Concept Plan Area, focus areas and detailed conceptual designs.
- ◆ **Chapter 4, Design Guidelines:** Guidelines for development on Bay Street, Main Street and for other parts of Irvington.
- ◆ **Chapter 5, Plan Implementation:** Summary of possible changes that the City will use to implement the Plan.
- ◆ **Appendix A, Demographics and Market Analysis:** Detailed information on the de-

mographics/economic trends and real estate conditions in Irvington.

- ◆ **Appendix B, Sales Leakage Analysis:** The sales leakage analysis was used to determine the degree to which there is an unmet demand for retail goods in the Concept Plan Area.
- ◆ **Appendix C, Traffic Analysis:** A qualitative discussion of transportation-related issues and their potential implications with regard to conceptual designs for the Concept Plan focus areas.

INTRODUCTION

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I EXISTING CONDITIONS

This chapter summarizes existing conditions in the Irvington Concept Plan Area as of the time period during which this Concept Plan was developed (2001-2004). It includes a summary of the *Urban Design and Development Existing Conditions*, prepared in September, 2001. That report detailed existing urban design, market and transportation conditions in Irvington.

A. Public Policy

This section includes a description of existing General Plan designations and policies, zoning requirements and existing land uses and conditions.

1. General Plan Designations and Policies

Figure 1-1 illustrates existing General Plan designations in the Concept Plan Area. The primary land use designation in the Concept Plan Area is Community Commercial, which applies in all of Irvington's central core.

General Plan policies that are particularly relevant to development of the Concept Plan include:

- ◆ Land Use Element Policy 2.14 states that the City will seek to maintain an active pedestrian environment. Buildings within Community Commercial designated area are encouraged to be oriented towards sidewalks or public plazas and walkways, with retail encouraged at the ground level.
- ◆ Land Use Element Policy 2.15 encourages developments to provide safe, convenient and continuous pedestrian walkways linking building entrances to street sidewalk crossings and activity centers where appropriate.
- ◆ Land Use Element Policy 2.16 states that development guided by a plan should identify parking strategies for the commercial area as a whole so as to foster a pedestrian-oriented shopping environment.
- ◆ Land Use Element Policy 2.19 states that the design, scale and character of Community Commercial parcels should be preserved with revitalization and development plans.
- ◆ Local Economy Element Policy 3.4.2 encourages medium intensity commercial and office areas around the Irvington BART station site.
- ◆ Implementation Program 19 of the City's Housing Element encourages redesignation and rezoning of parcels to accommodate medium- to very- high- density housing developments within proximity of transit services such as BART, Alameda/Contra Costa (AC) Transit and Santa Clara Valley Transit Authority (SCVTA).

This Concept Plan seeks to support all of these policies by creating a more vibrant commercial core in Irvington and an intensification of land uses adjacent to potential transit services.

CHAPTER 1: EXISTING CONDITIONS

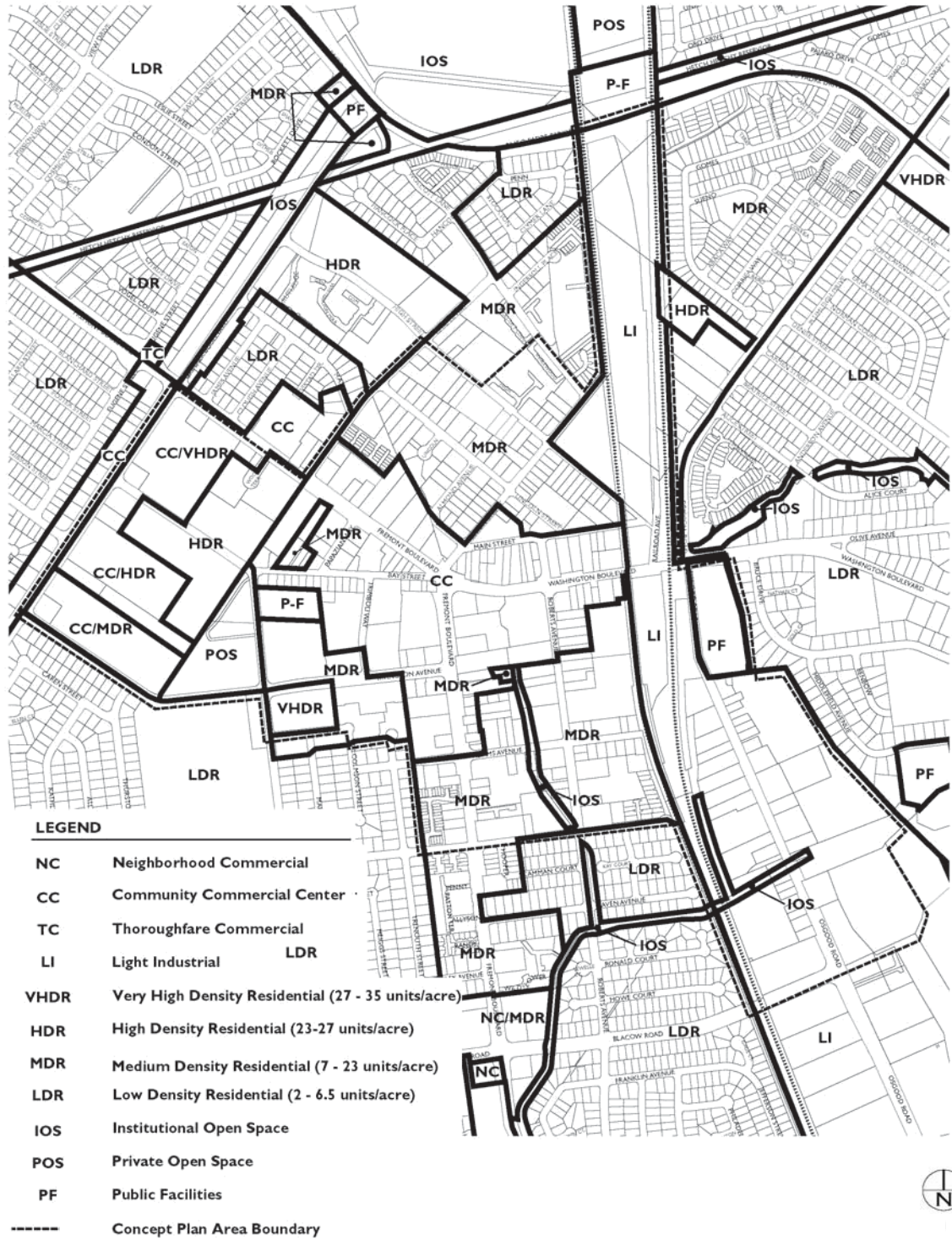


Figure 1-1. General Plan Designations

LEGEND

C-N	Neighborhood Commercial
C-C(I)	Community Commercial Irvington Overlay
C-T	Thoroughfare Commercial
C-G	General Commercial
I-L	Light Industrial
P	Planned District
R-G	Garden Apartment Residential
R-2	One and Two Family Residential
R-I	Single Family Residential
O-S	Open Space

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2. Zoning

The zoning designations in the Concept Plan Area are consistent with the General Plan designations. The zoning designations are illustrated in Figure 1-2.

- ◆ **Community Commercial Irvington Overlay (C-C(I)).** This is the largest district within the Concept Plan Area. It allows retail and services and does not permit wholly residential projects. The maximum allowed floor to land ratio (FAR) is .5.
- ◆ **Garden Apartment Residential (R-G/R-3).** Uses allowed in this district include apartment buildings, manufactured and mobile homes, residential care facilities and small family day care homes.
- ◆ **Planned District (P).** Planned districts allow for a both residential and commercial uses and are generally subject to zoning regulations most closely associated with the proposed use. Residential components of mixed-use developments in Planned Districts are restricted to being less than 49 percent of the floor area of the development. Development in these districts generally follows the adoption of a precise site plan. Irvington has a number of Planned District projects for which site plans have been approved but are not yet under construction.

B. Existing Land Uses

Irvington has a varied range of existing land uses all within a close proximity of Five Corners, as illustrated in Figure 1-3.

- ◆ **Retail.** Uses include both large and small businesses and restaurants.
- ◆ **Office.** Uses include small offices offering a range of professional services.
- ◆ **Commercial.** Uses include both retail and office components.
- ◆ **Industrial.** Uses include storage, warehouse and construction-related operations on parcels along the railroad tracks.
- ◆ **Multi-Family Residential.** Uses include a variety of multi-family neighborhoods in relatively close walking distance of Irvington's commercial core.
- ◆ **Single-Family Residential.** Uses include single-family neighborhoods in relatively close walking distance of Irvington's commercial core.

CHAPTER 1: EXISTING CONDITIONS

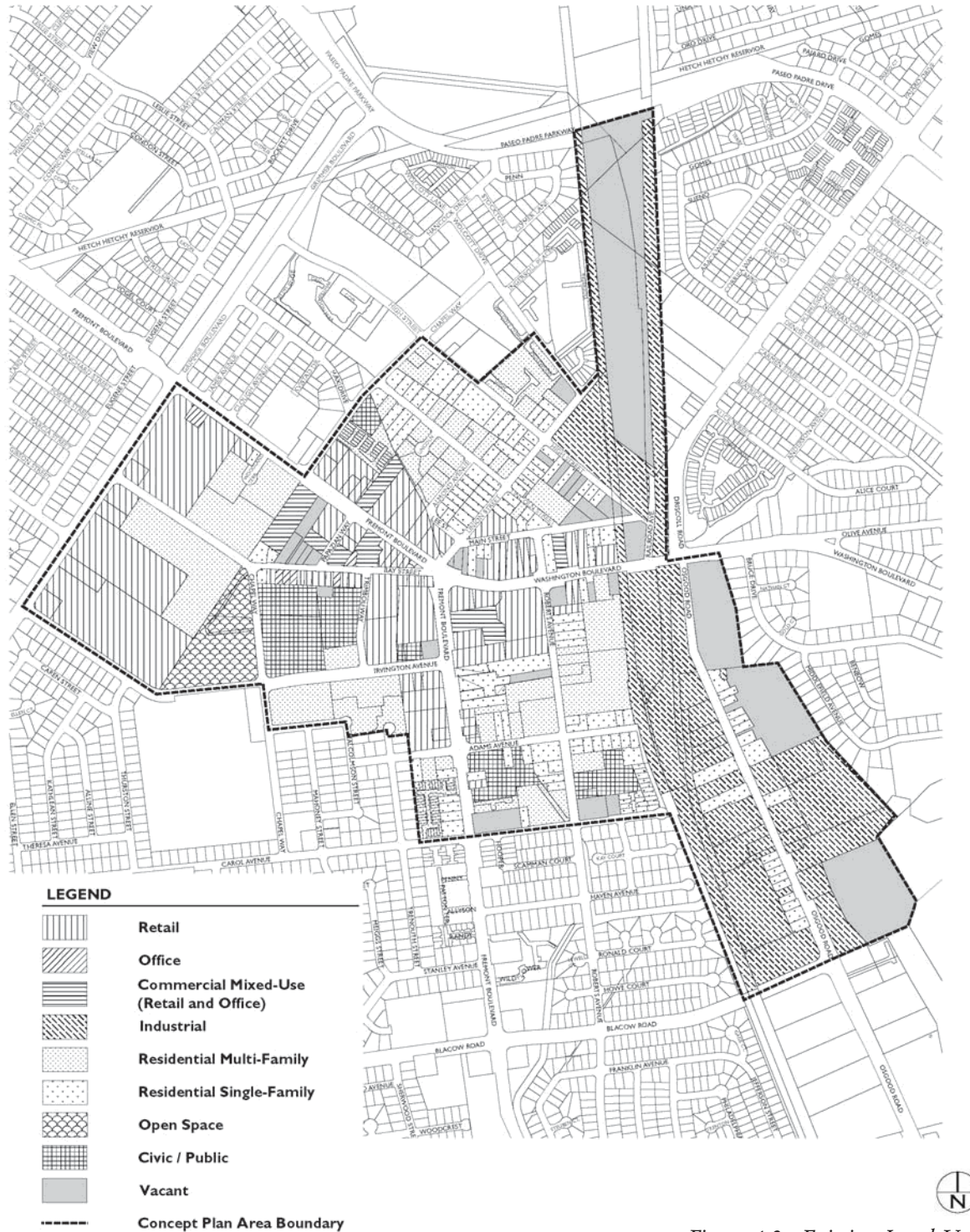


Figure 1-3. Existing Land Uses

C. Urban Design

The Concept Plan Area has developed with a range of both automobile- and pedestrian-oriented uses. The existing built environment of Irvington has many characteristics that can be emulated in order to achieve the General Plan directive that future development in Irvington should “foster economic vitality and a focused, pedestrian-oriented commercial center.”

1. Streets

Figure 1-4 illustrates two types of streets in the Concept Plan Area.

- ◆ **Arterial Streets.** Fremont Boulevard and Washington Boulevard are the primary vehicular corridors for moving traffic through Irvington. Past investments on these streets include the palm trees along Fremont and Washington Boulevards; paving, sidewalks, plazas and columns at Five Corners; and the Irvington gateway signs.
- ◆ **Pedestrian Streets.** Many other streets operate tangentially to the arterials. These streets, namely Bay, Union and Main Streets, offer walking opportunities for connecting existing and potential development to the center of Irvington at Five Corners.

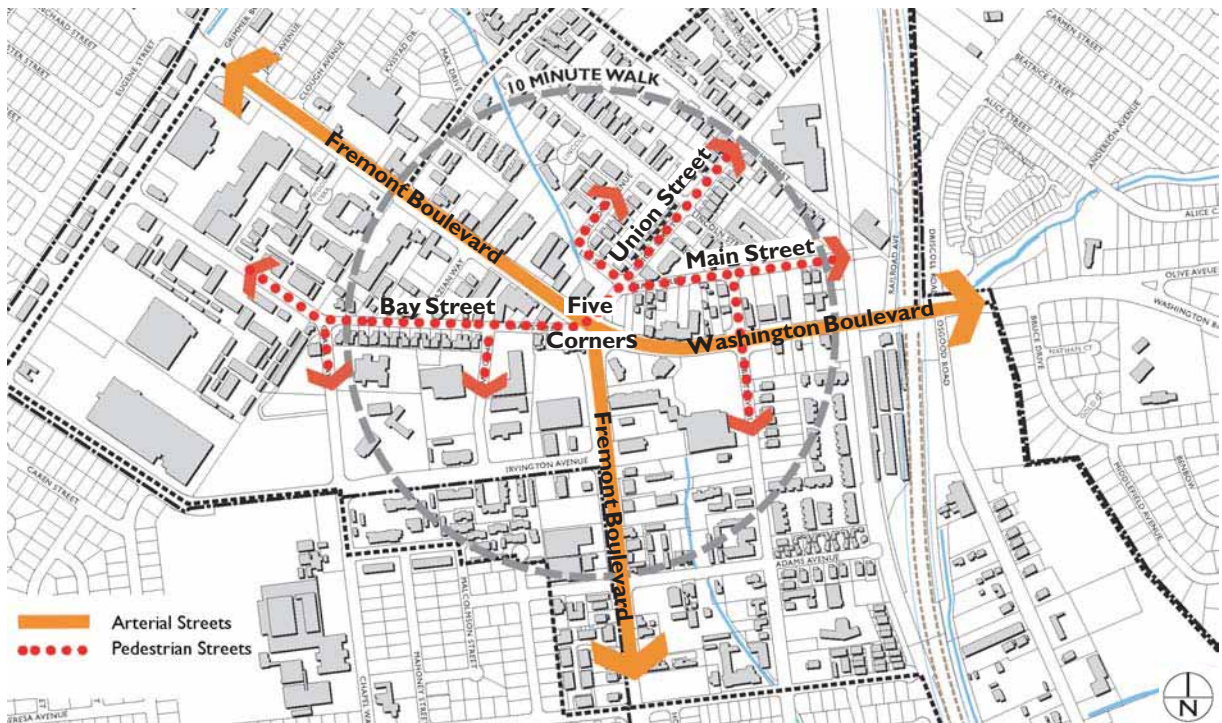


Figure 1-4. Existing Street Network at Five Corners

Figure 1-4 illustrates the ability to use any combination of Irvington streets to access a large part of the Concept Plan Area within about a ten-minute walk. The existing network of streets offers the opportunity for future development to be pedestrian-oriented, while also continuing to provide appropriate vehicular circulation through the area.

2. Sidewalks and Pedestrian Amenities

In a pedestrian-oriented environment, logical and safe pedestrian facilities should provide connections to all destinations while minimizing conflicts with vehicles. Most of the commercial streets in the Concept Plan Area have good sidewalks and well-maintained landscaping. Sidewalks on Bay Street between Fremont Boulevard and Chapel Way have been designed to be developed with pedestrian amenities, such as pedestrian-scale lighting, landscaping buffers, curb bulbs, plaza area, and street furniture. The Irvington Monument Plaza at Five Corners contains street furniture and other pedestrian amenities. Street light fixtures on Bay Street in this area are scaled to a pedestrian, which creates a pleasant walking environment and creates a night time lighted environment that can allow Bay Street users to feel safe. However, a majority of the Concept Plan Area has lighting that is primarily oriented towards vehicles and the night time needs of automobile drivers.

The major pedestrian constraint that exists within the residential area of the Irvington District is the lack of sidewalk continuity and consistent sidewalk standards. This is also evident on commercial streets such as Main Street and Bay Street. Design concepts in this Plan will extend sidewalk continuity and consistent standards throughout the Irvington District area. Additionally, the City has a number of pending street improvement projects underway. These pending improvements were taken into account during the planning process.

3. Buildings

There are a number of well designed pedestrian-oriented buildings in the Concept Plan Area. The scale of architectural design and building materials can be emulated by future development in Irvington to create a distinct identity for the district. At Five Corners, the Bay Street buildings that face onto Irvington Monument Plaza generally have a high level of architectural detailing that helps to create a comfortable scale and environment for pedestrians. Front windows of these buildings are generous, allowing passersby the opportunity to see in and window shop.

4. Residential Neighborhoods

Surrounding Five Corners are a number of established residential neighborhoods. These neighborhoods are comprised of both single-family and multi-family residences. Many of the homes and apartments in the area are well taken care of.

The Concept Plan builds upon these already evident aesthetic features to define future development in Irvington that creates vital, safe and walkable commercial and residential streets and neighborhoods.

D. Seismic Considerations

Irvington is bisected by the Hayward Fault, which is an active earthquake fault with a strong near-term potential to experience a major earthquake. This means that the entire area is subject to strong ground shaking, while a corridor along the fault itself is also subject to ground rupture in an earthquake. Under the State's *Alquist-Priolo Earthquake Fault Zoning Act*, land around active earthquake faults is required to be in a geologic study zone.

The geologic study zone is a broad area on either side of an approximately located fault. Figure 1-5 illustrates the approximate location of the Hayward Fault and its geologic study zone. The Alquist-Priolo Act requires that any property owner proposing a project within the geologic study zone must conduct a geologic investigation to pinpoint the location of the fault. Once the location of the fault has been more accurately

identified, habitable structures must be setback from it. Thus the geologic study zone shown in Figure 1-5 does not represent an area where building is precluded; it only represents an area requiring study. Generally, a minimum 100-foot wide area within this study zone will most likely be off limits to structures as determined by a geotechnical engineer.

This Concept Plan assumes that the Hayward Fault runs in the alignment shown in Figure 1-5. Conceptual designs illustrated in the Concept Plan maintain a 50-foot setback for all development on both sides of the fault. This setback area will need to be adjusted as specific developments are proposed and the exact location of the fault and the actual setback requirements are pinpointed.

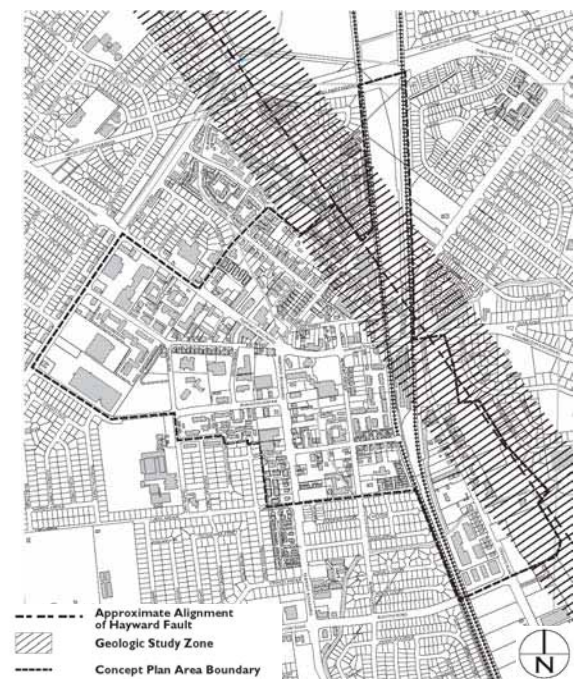


Figure 1-5. Hayward Fault

E. Market Conditions

This section summarizes key findings from three economic reports that were completed by economic consultants during the Concept Plan's development. The three reports include:

- ◆ *Demographics/Economic Trends and Real Estate Market Conditions Memorandum*, by Bay Area Economics (BAE). This report is included in Appendix A. BAE is an independent consultant that provides comprehensive real estate economic analysis and urban development economics.
- ◆ *Sales Leakage Analysis for Irvington District Memorandum*, by BAE and included as Appendix B.
- ◆ *Fremont Retail Market Assessment and Downtown Retail Strategy Study*, presented to the Fremont City Council in April, 2002, by Thomas Consultants.
- ◆ Most retail space in the Concept Plan Area serves primarily the local area, and does not attract users from outside Irvington.
- ◆ Drugstore sales in Irvington are not as high as they could be. Irvington could potentially support a full-size chain drugstore, such as Long's or Walgreen's.
- ◆ Although there is little or no unmet demand for a typical large supermarket, there may be some potential for a smaller, specialty grocery store.
- ◆ Restaurants are a strong existing sector and currently attract sales from both inside and outside the Trade Area. If Irvington's strong restaurant base represents a "cluster" that would attract other operators, Irvington could possibly attract additional restaurant establishments.
- ◆ Local-serving and multi-cultural retailers are recommended Irvington tenants.

1. Retail

Key findings related to retail conditions in Irvington include:

- ◆ Current retail lease rates are lower in the Concept Plan Area relative to the Central Business District.

2. Residential

This section compares trends in the Concept Plan Area, the Trade Area and the City as a whole. The Trade Area is the geographic area that generates the majority of the customers for a community. For the Concept Plan Area the Trade Area is defined by a two-mile radius surrounding Five Corners. Sales data from 2001 indicate the following:

- ◆ On average, single-family homes in the Trade Area are smaller and less expensive than single-family homes sold Citywide.
- ◆ The Concept Plan Area has a population that is younger than other parts of Fremont and more likely to live in renter households.
- ◆ The Trade Area contains a more affluent population than the Concept Plan Area, with larger households that are more likely to become homeowners.
- ◆ In August 2001 rental units were at or very near full occupancy. Over the past few years, rental vacancy rates citywide have risen to 5.4% (Real Facts survey (First Quarter of 2004)).

3. Office

Although Fremont is included in the Silicon Valley office market, there is very little office space in the Concept Plan Area, and demand for office space seems to be low as well. The low demand in the Concept Plan Area is likely attributable to the location of larger office complexes nearby in the Warm Spring area and the CBD. However, there may be sufficient unmet demand in the Concept Plan Area to support some office space as part of mixed-use projects.

F. Planned Public Improvements

Figure 1-6 shows public improvements already planned or underway in Irvington. These public projects were examined during the planning process and the Concept Plan includes design solutions for these key project sites.

- ◆ **Bay Street Streetscape and Utilities Undergrounding.** This project will provide the opportunity for redesign and re-configuration of curb cuts and sidewalk width along Bay Street from Chapel Way to Five Corners, and ultimately improve the entire visual aesthetic of the street.
- ◆ **Washington Boulevard Grade Separation and Railroad Realignment.** An overpass is planned for Washington Boulevard that will cross over the rail lines. Parcels on either

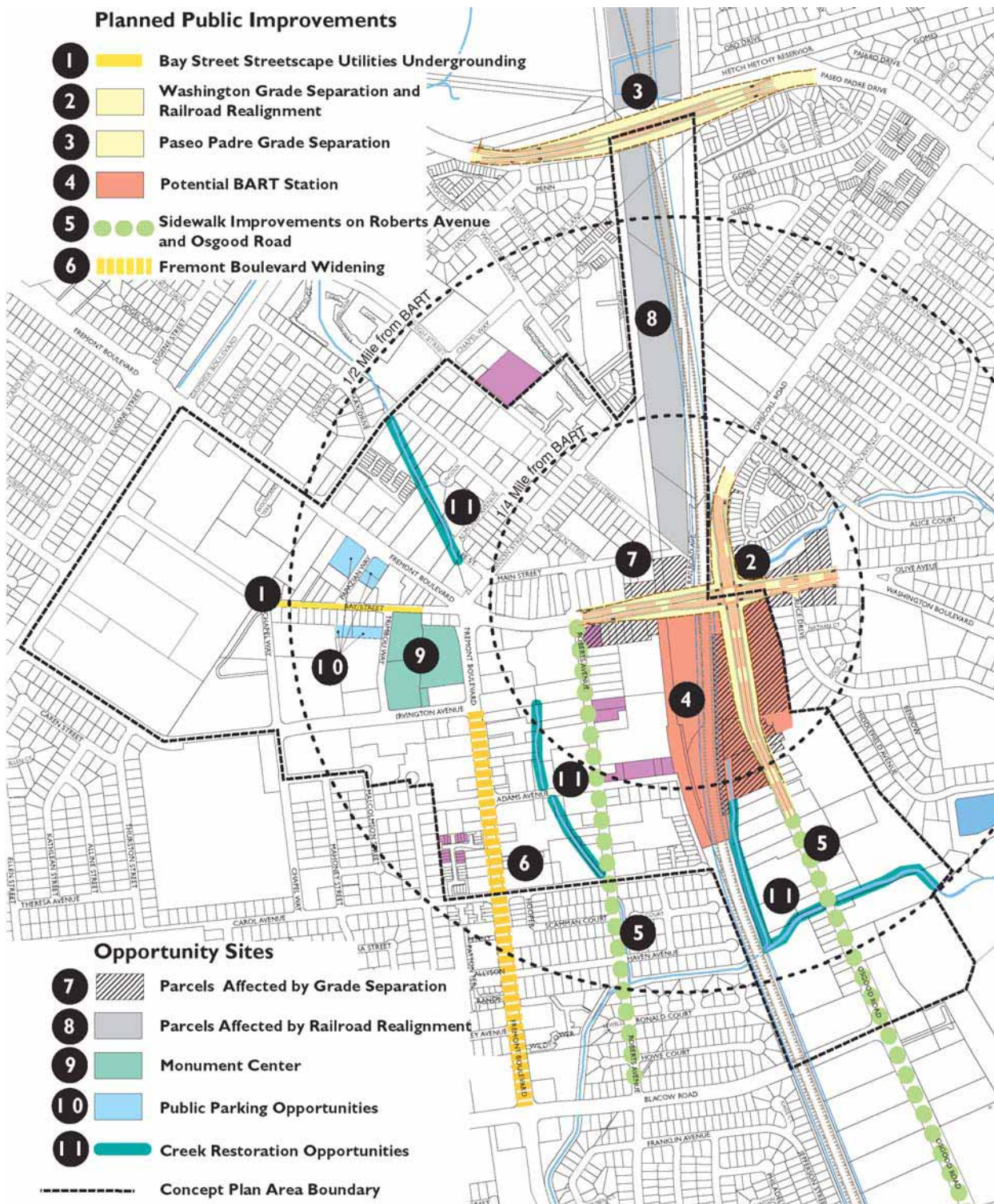


Figure 1-6. Planned Public Improvements and Opportunity Sites

side of Washington Boulevard between Roberts Avenue and the railroad tracks will lose access to Washington Boulevard, due to the grade separation.

- ◆ **Paseo Padre Grade Separation.** An underpass is planned for Paseo Padre Parkway that will take traffic under the Union Pacific Railroad tracks. The project will include a realignment of the most westerly of these rail lines. The most easterly rail line is anticipated to be abandoned.
- ◆ **Potential Future BART Station.** A potential future BART station in Irvington is planned but unfunded at this time.
- ◆ **Sidewalk Improvements on Roberts Avenue and Osgood Road.** The City has projects to complete sidewalks on both Roberts Avenue and Osgood Road. In addition, the City has plans to widen Osgood Road to four lanes.
- ◆ **Fremont Boulevard Widening.** The City is in the process of widening Fremont Boulevard from Irvington Avenue to Blacow Road.

G. Opportunity Sites

There are a number of key opportunities in the Irvington District which are shown in Figure 1-6. These opportunities were examined during the planning process and the Concept Plan includes design solutions for these key opportunity sites.

- ◆ **Parcels Affected by Grade Separation.** Parcels on both sides of Washington Boulevard between Roberts Avenue and the railroad tracks, including parcels facing Main Street, will be affected by the Washington Boulevard Grade Separation Project. Some of these parcels will have their existing buildings removed. This will provide opportunities for new development in the area.
- ◆ **Parcels Affected by Railroad Realignment.** The railroad track realignment will create better accessibility to parcels along the railroad between Washington Boulevard and Paseo Padre Parkway.
- ◆ **Monument Center.** The Monument Center shopping center at the corner of Fremont Boulevard and Bay Street has been the subject of several redevelopment proposals in the past. Although the City is no longer pursuing publicly- financed redevelopment on this site, the site's size and current uses make it a possible location for new private sector development.

- ◆ **Public Parking Opportunities.** The City is currently working with property owners near Bay Street to provide additional public parking in this area.
- ◆ **Creek Restoration Opportunities.** There may be portions of Laguna Creek that could be improved to become open space amenities in Irvington. This would particularly be true in Irvington's residential neighborhoods.

H. Existing Utilities and Easements

There are a number of existing and proposed utility line easements within the Union Pacific railroad right-of-way. While the existing Union Pacific rail line will be relocated easterly to facilitate the Grade Separation Projects at Paseo Padre Parkway and Washington Boulevard the existing utility line easements for telecommunication facilities and other utilities that will remain in place. In addition, new easements may also be needed to allow for other utilities that need to be relocated as part of the Grade Separation project.

CHAPTER 1: EXISITING CONDITIONS

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2 PLAN VISION AND GOALS

This chapter outlines the vision and goals for Irvington's central core that this Concept Plan seeks to realize.

The vision and goals were developed in workshops attended by Irvington community members, local business owners, the consultant team and City staff. All the development concepts, design guidelines and implementation measures in subsequent chapters of this Concept Plan are intended to provide a potential path for the City to make the vision and goals of this Chapter a reality.

A. *The Vision for Irvington*

Irvington will be a walkable neighborhood with shopping and dining opportunities available to both local residents and the larger community. The heart of Irvington is the Five Corners. The historic character, commercial opportunities and pedestrian scale of the Five Corners area will provide the impetus for future development throughout Irvington.



Figure 2-1. *Perspective Illustration looking toward Five Corners from Bay Street*

New commercial development in Irvington will include unique shops and restaurants offering a range of choices to residents of the district, the city of Fremont and the region. Stores and offices located on Irvington's commercial streets will be enhanced by well-designed pedestrian amenities such as landscaping, attractive sidewalks, benches and street lamps. These features will contribute to safe and walkable streets.

A new BART station near Washington Boulevard will be integrated into Irvington's overall fabric to be a positive influence on new development in the district, particularly on those areas in close walking proximity of the station. New commercial development, shops and housing related to the BART station will be developed in keeping with the overall vision for Irvington.

B. Concept Plan Goals

Goal 1: Develop an identity for Irvington that reflects Irvington's history and distinguishes it from other Fremont districts.

Goal 2: Improve the overall appearance of the neighborhood and commercial district.

Goal 3: Encourage revitalization of underutilized parcels and buildings, including historic buildings and neighborhood features.

Goal 4: Attract unique shopping, dining and neighborhood services to Irvington.

Goal 5: Provide an integrated, safe and well-designed pedestrian and bicycle network, including access to Laguna Creek, Central Park and other recreational amenities.

Goal 6: Minimize the impact of vehicular through-traffic to the degree feasible while meeting circulation needs.

Goal 7: Improve streetscape design in Irvington.

Goal 8: Develop parking facilities that meet commercial and residential needs.

Goal 9: Improve bus service and scheduling to meet the needs of Irvington residents.

Goal 10: Integrate a potential future BART station and accompanying residential and commercial development into Irvington.

Goal 11: Add housing to central Irvington in a way that respects Irvington's role as one of Fremont's important commercial cores.

3 DEVELOPMENT CONCEPTS

This chapter describes and illustrates the development concepts for Irvington that could be used (upon implementation in accordance with Chapter 5) to achieve the vision and goals described in Chapter 2. This chapter includes an overview of the development concept for the entire area, as well as descriptions of eight “focus areas” which received significant attention during the planning process.

The overall development concept for Irvington, illustrated in Figure 3-1, is to strengthen the district’s center at Five Corners, building upon the historic character, commercial and residential opportunities and potential for pedestrian-scale development inherent in the district. Bay Street and Main Street, linked by Five Corners and a short segment of Union Street, will become the primary pedestrian-oriented corridor in the district, linking the Monument Center and existing Bay Street retail to a new BART station. This corridor will become a distinctive commercial pedestrian amenity for the Irving-

ton district while also emphasizing the BART station role as a neighborhood station augmented by regional patrons arriving by automobile via Osgood Road.

A fundamental principle in the development of the Concept Plan is that Fremont Boulevard and Washington Boulevard need to continue to function as significant vehicular arterial roadways. Bay Street, Union Street and Main Street extend from these arterials and can be developed to facilitate pedestrian linkages from Five Corners to all the major parts of the area and the potential future BART station. This pattern of development, in which pedestrian-oriented commercial streets extend from major arterials, is found in many successful models throughout the Bay Area.

The remainder of this chapter looks at new development under this concept in eight “focus areas”, which are mapped in Figure 3-2.

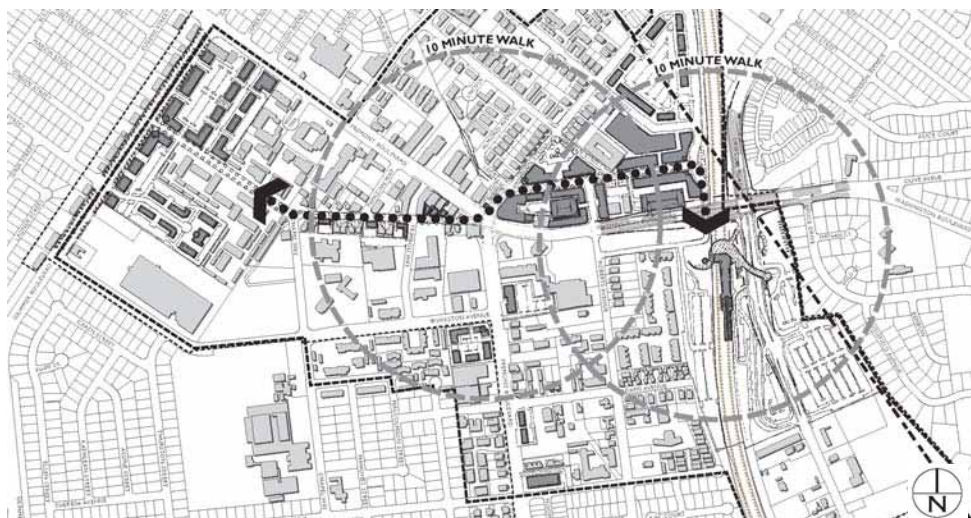


Figure 3-1. Overall Development Concept

CHAPTER 3: DEVELOPMENT CONCEPTS

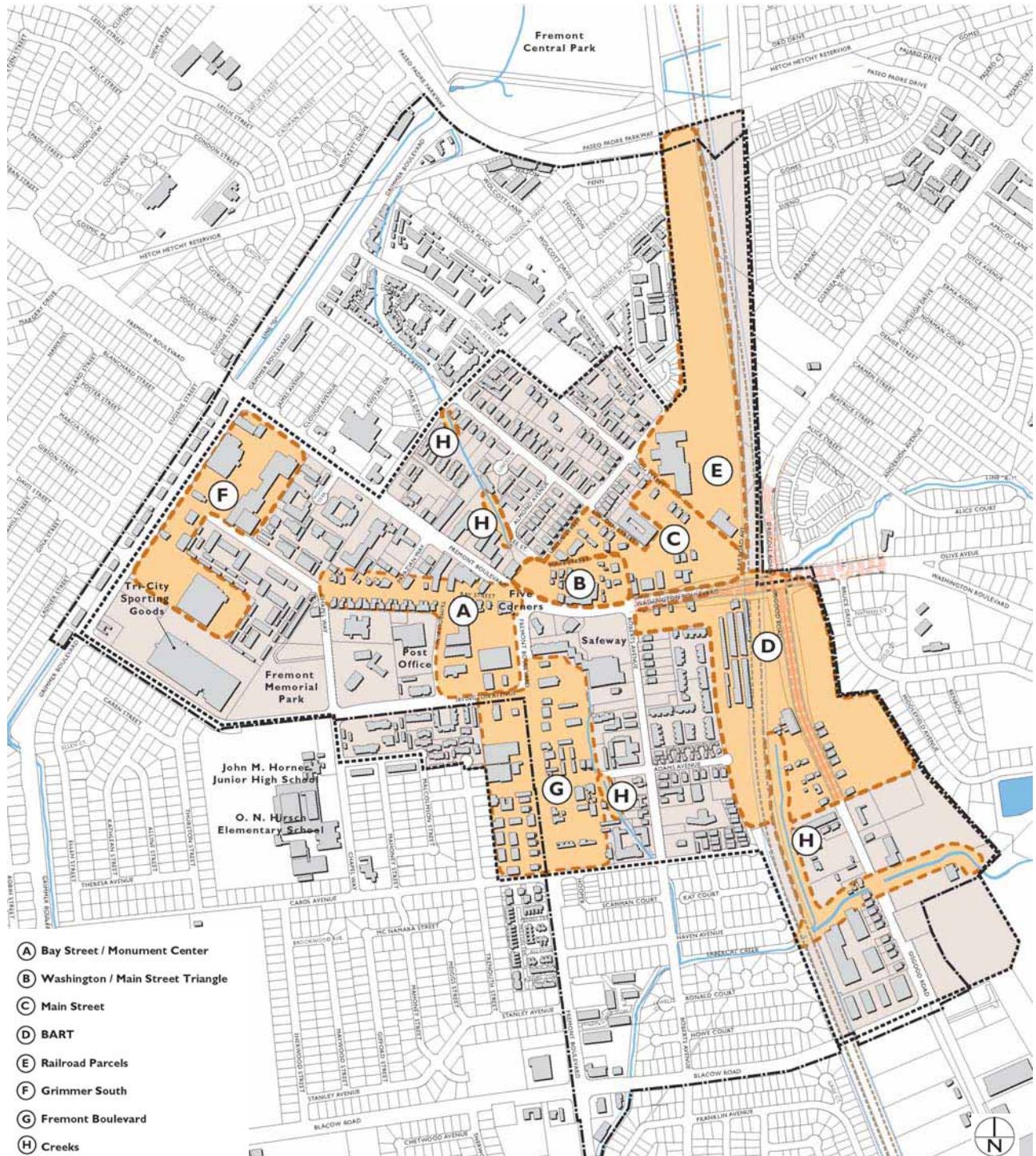


Figure 3-2. Focus Areas Diagram

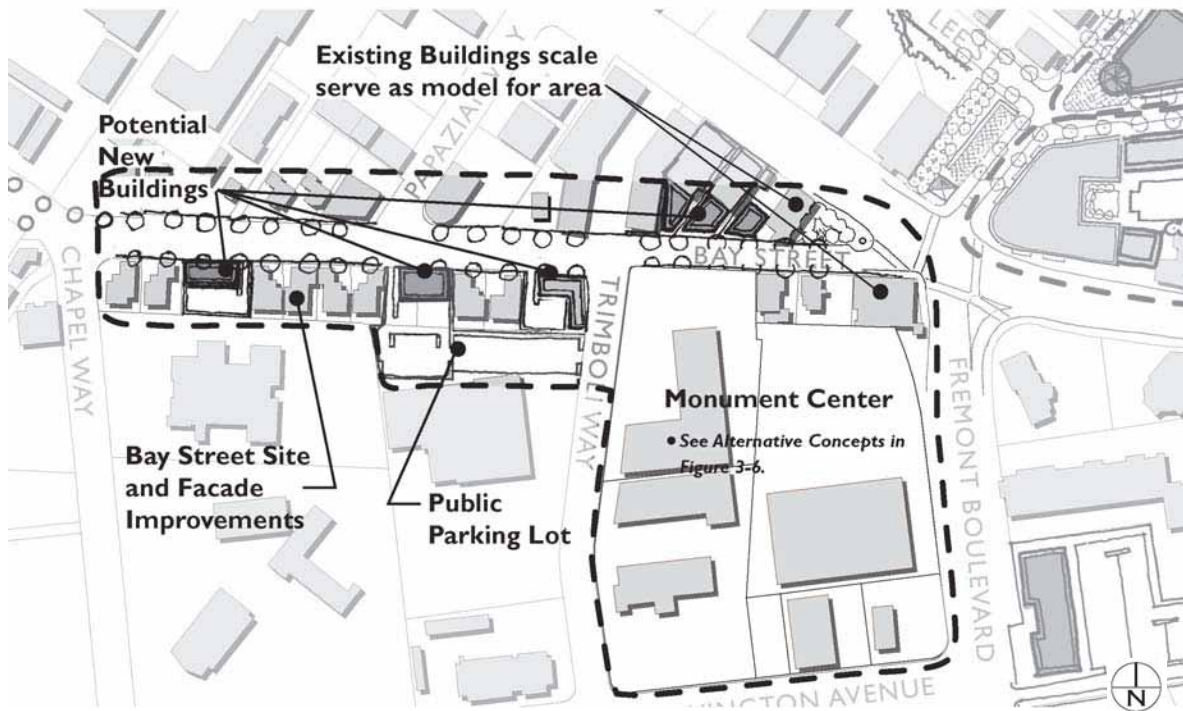


Figure 3-3. Bay Street/Monument Center Concept

A. Bay Street/Monument Center

This focus area includes a two-block segment of Bay Street between Fremont Boulevard/Five Corners and Chapel Way, as well as the 6-acre Monument Center site. Bay Street is characterized by a series of 1950's single-family homes which have been converted to commercial uses, with residences remaining behind. Front yards of these homes have been transformed into parking areas. The Monument Center is also included because it is a large site at the center of Irvington with direct connection to Bay Street.

1. Bay Street Concept

The RRM Design Group has developed the *Bay Street Schematic Design Concept*, which is intended to make Bay Street a more vibrant and pedestrian-friendly retail corridor in Irvington that builds on the scale and character of the buildings that front onto the Five Corners. Site and facade improvements foreseen in this plan are intended to produce a more unified character for the street and offer pedestrian-scale amenities such as awnings, trellises and landscaping. Parking spaces in areas originally used as residential front yards on the small lots between Chapel Way and Trimboli Way would be relocated to diagonal parking arrangements on the street or to a parking facility in close proximity to Bay Street.



Figure 3-4. Vision for Bay Street

The streetscape improvements envision undergrounding of utilities, landscaping, crosswalks, street widening, lighting and street furniture. The design schematic developed for Bay Street is illustrated in Figure 3-5A and 3-5B. Facade and front setback improvements to commercial establishments can be implemented by individual property owners. Improvements such as entry plazas, semi-private outdoor space and landscaping or short fences to define the transition between the sidewalk and the business are also allowed.

Any new private redevelopment of parcels on Bay Street will be required to build to a zero setback and have ground floor commercial uses. This typology will follow the precedent set by the buildings on the south side of Bay Street adjacent to Five Corners, illustrated in Section C-C of Figure 3-5B. Similarly, Figure 3-3 shows a few new buildings on Bay Street. Property owners may find that other parcels on the street are appropriate for redevelopment in a similar manner.

A view of the overall completed vision for Bay Street is shown in Figure 3-4.

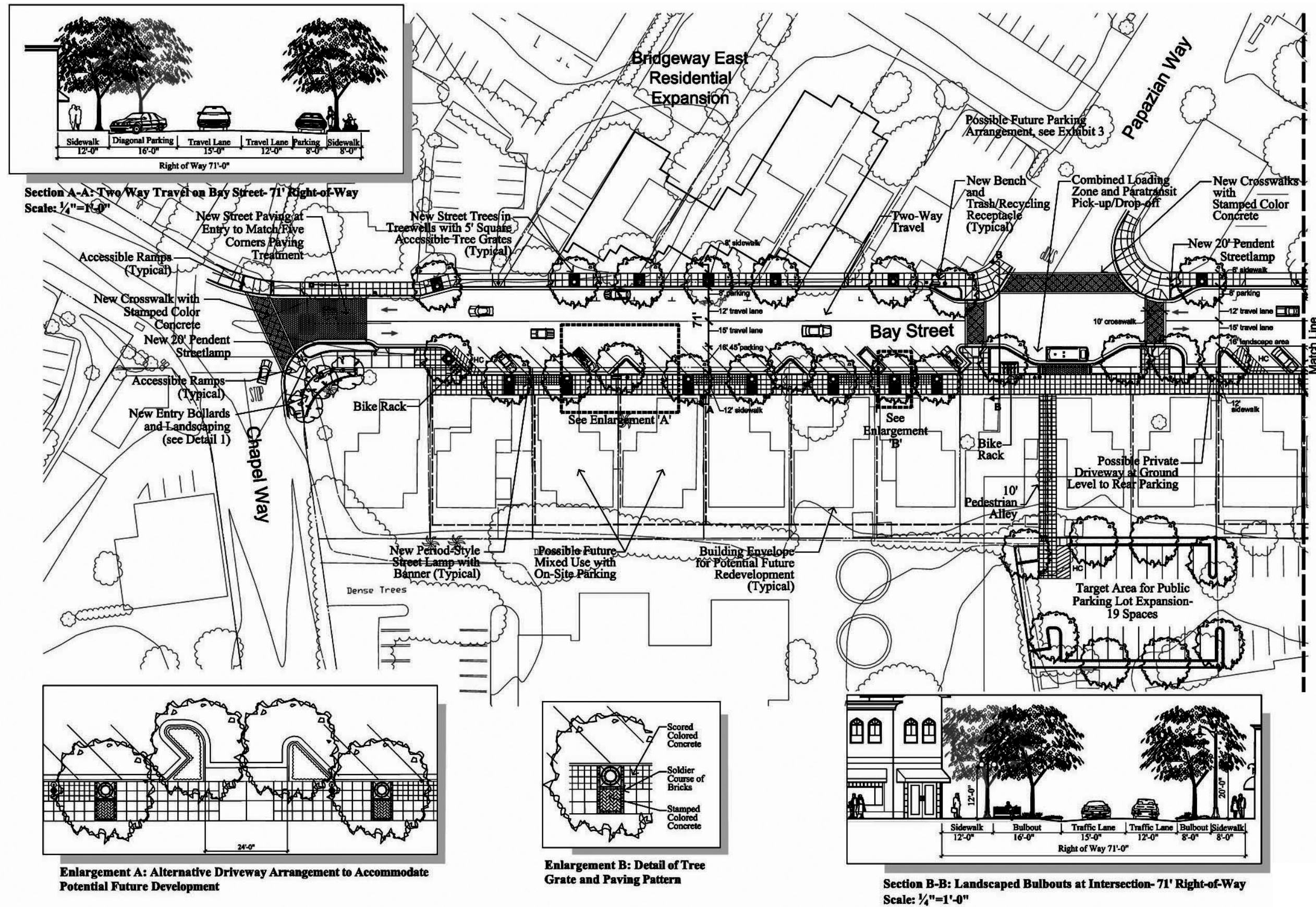
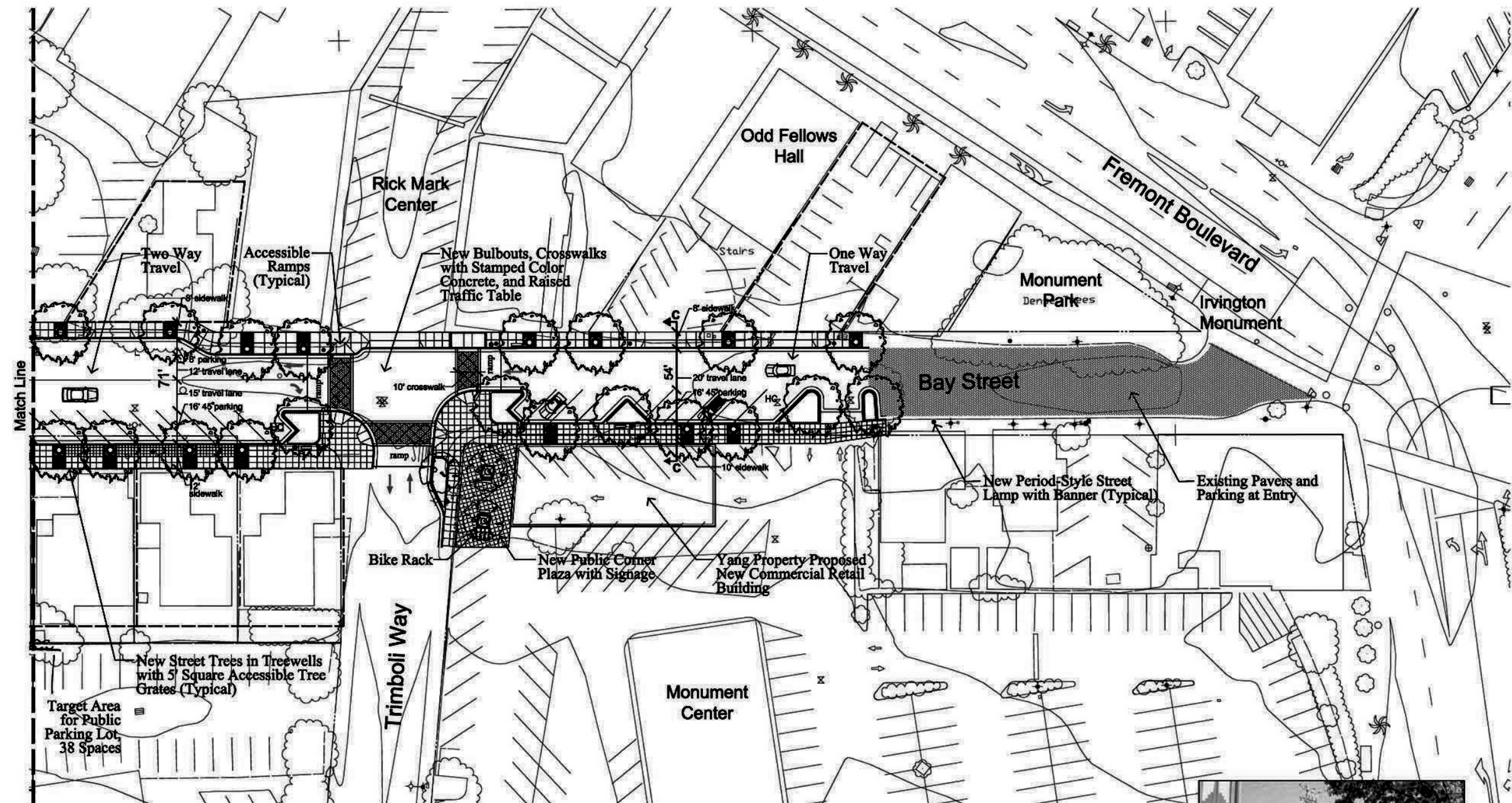
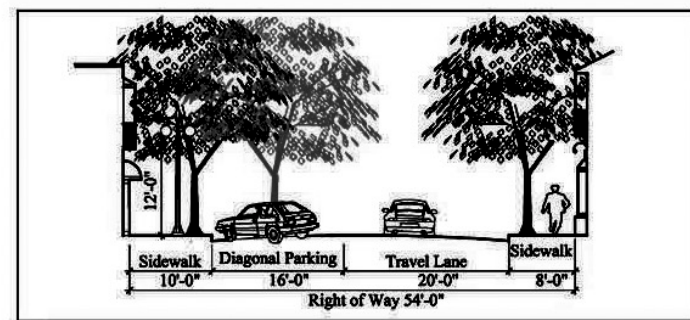


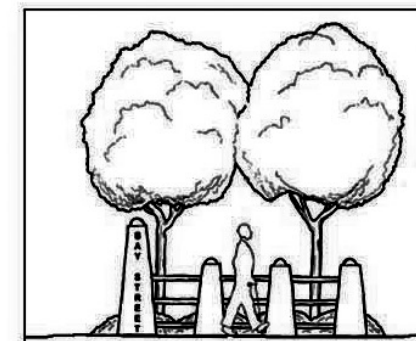
Figure 3-5A. Bay Street Schematic Design Concept (West)



Existing Parking on Bay Street:
46 parallel spaces
Proposed Parking on Bay Street:
12 parallel spaces
+44 diagonal spaces
56 total spaces



Section C-C: One Way Travel on Bay Street- 54' Right-of-Way
Scale: $\frac{1}{4}''=1'-0''$



Detail 1: View of Entry Bollards and Landscaping; Similar to Bollards at Monument Park



Monument Park



RRM DESIGN GROUP

2. Monument Center

This site forms the southern street frontage of Bay Street between Trimboli Way and the historically significant buildings opposite the plaza. The entire site, which is also bounded by Fremont Boulevard, Irvington Avenue and Trimboli Way, is key to the future of Irvington and Five Corners.

Appropriate development of this site can have a significant and positive influence on Bay Street. Objectives for the site include a retail and pedestrian-oriented environment on Bay Street, an overall commercial orientation, a mix of uses, additional parking to serve adjacent businesses, and the use of high quality design and materials.

Three general development alternatives for the Monument Center Site could include:

- ◆ **Alternative 1.** Figure 3-6A illustrates redevelopment of the Monument Center by rehabilitating and reconfiguring the existing buildings on site.
- ◆ **Alternative 2.** Figure 3-6B illustrates redevelopment of the Monument Center site into 100% commercial use allowing visual and parking access off Fremont Boulevard.
- ◆ **Alternative 3.** Figure 3-6C illustrates redevelopment of the Monument Center into 50% commercial use and 50% residential use. This alternative could also include a new road through the site from Fremont Boulevard to Trimboli Way, allowing for visual and parking access from Fremont Boulevard.



Figure 3-6A.
Alternative 1:
Building
Rehabilitation
and Site
Reconfiguration

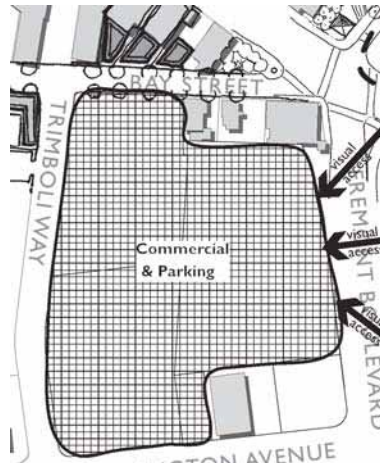


Figure 3-6B.
Alternative 2:
New Commercial
Development

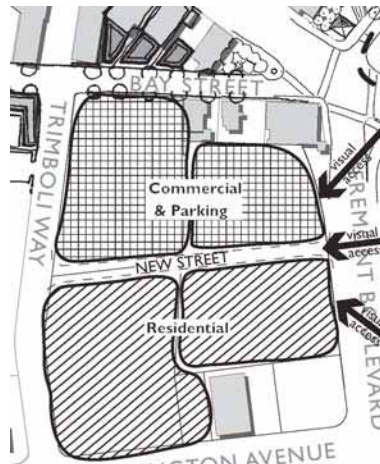
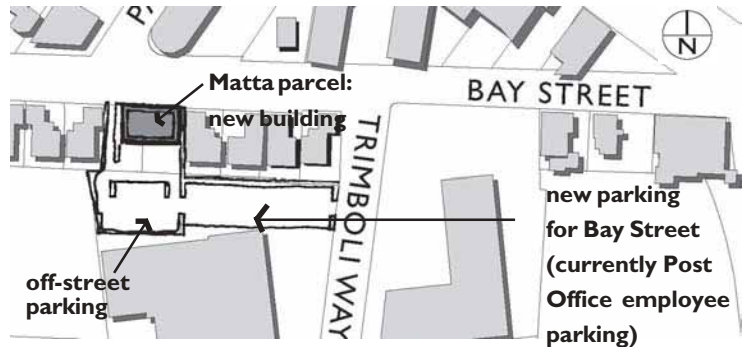


Figure 3-6C.
Alternative 3:
New Mixed Use
Development

Figure 3-7. This concept for the Matta parcel envisions a new building with parking serving all of Bay Street behind it. An opportunity may also exist to utilize existing Post Office parking to serve Bay Street. Any design for the Matta parcel could provide for a future connection to the Post Office parking lot.



3. Parking

Redevelopment of Bay Street parcels and implementation of streetscape improvements would cause a net loss of on-site parking spaces. The schematic design plan shown in Figure 3-5A and Figure 3-5B shows diagonal parking along Bay Street, which would increase on-street parking. Future development on Bay Street may require the development of some additional parking in the immediate vicinity. To do this, the City will explore the creation of a common parking area to serve Bay Street.

As shown in Figure 3-7, the possibility of a new building on the Matta Parcel on Bay Street would increase the on-site parking. A possible private driveway at ground level would access the rear parking lot. The Matta parcel development could be designed to allow a connection to the Post Office parking. In the future, the Post Office parking could be moved elsewhere.

In addition, new development on the Monument Center site could incorporate a parking facility that provides adequate parking for its own de-

velopment program while also providing additional parking capacity for Bay Street businesses. This parking facility should be in close proximity to the intersection of Bay Street and Trimboli Way.

Finally, other opportunities could exist either side of the intersection of Papazian Way and Fremont Boulevard

4. Circulation

Currently Bay Street operates one-way west bound from Five Corners until west of the Monument Plaza and two-way traffic from that point to Grimmer Boulevard. Vehicles are allowed to enter Bay Street at Five Corners but cannot exit. Previous studies have evaluated extending this one-way pattern through to Chapel Way as well as returning the entire street to two-way traffic. The Bay Street plan proposes one way traffic heading west to Trimboli Way, returning to two-way traffic at the intersection of Bay Street and Trimboli Way.

B. Washington/Main Street Triangle

The Washington/Main Street Triangle focus area is a block bounded by Washington Boulevard and Main Street on the long sides and Union Street and Roberts Avenue on the shorter sides. The block contains long narrow parcels that front both Washington Boulevard and Main Street. The site has prominent frontage at Five Corners.

1. Overall Concept

The development of the Five Corners intersection of Washington Boulevard and Union Street will create the link between Bay Street and Main Street, the new pedestrian corridor at the center of the Concept Plan Area. The Washington/Main Street block is strategically sited at Five Corners and extends east on Main Street towards additional residential and commercial mixed-use development. The Five Corners frontage of this block is key and needs to include a building that faces onto Five corners and commands a significant architectural presence. This building should be home to a use that contributes to the activation of the Five Corners connection between Bay Street and Main Street. Potential uses might include a museum, an art gallery, a restaurant or a popular retail tenant.

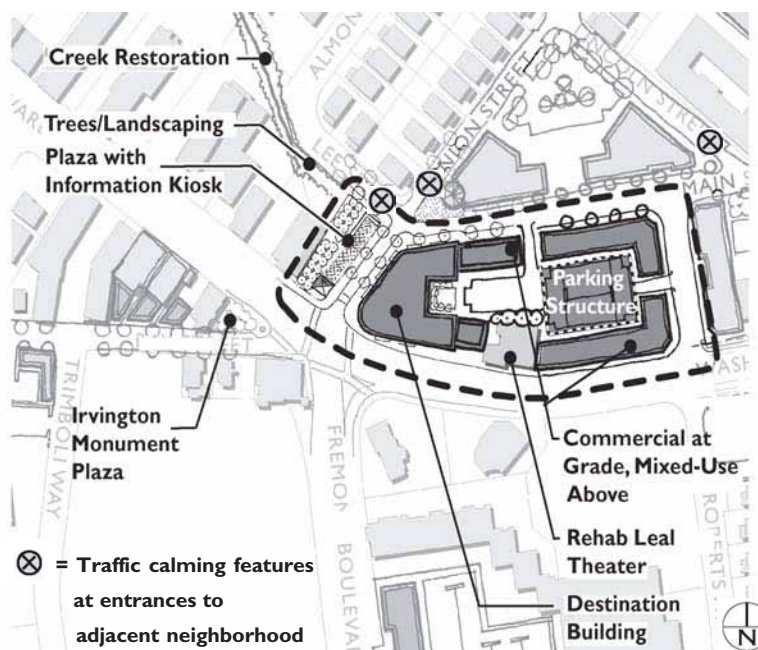


Figure 3-8. Washington/Main Street Triangle Development Concept

The Main Street frontage should be developed with ground floor commercial development with office or residential uses above. Development on Main Street and Roberts Avenue should reinforce pedestrian-oriented commercial activity. Development on Washington Boulevard should be set back further than existing development, due to higher traffic volumes on Washington Boulevard and allowing for wider sidewalks. The incorporation of street trees on Main Street would provide a buffer between pedestrians and vehicles.



Figure 3-9. A view across Five Corners from Bay Street

The site for a potential plaza, at the northwest corner of Union Street and Fremont Boulevard, is private land and would require a public action to implement. The site is constrained by an underground channelized creek. The plaza should be designed to tie into Irvington Monument Plaza across Fremont Boulevard at the entrance to Bay Street in order to facilitate a better link across Five Corners and connect the commercial activities of Bay Street to those of Main Street. As shown in Figures 3-9 and 3-10, the new corner plaza should provide public open space and outdoor seating.

Restoration of the historic Leal Theater on Washington Boulevard would help to implement Concept Plan goals for new development. The Leal Theatre embodies the architectural vocabulary and details seen on historic Irvington buildings that the goals of the Concept Plan are based on. Adjacent buildings should take cues from the Leal Theater's architectural features. The Leal Theater could also be utilized for other purposes, including upper floor residential.

CHAPTER 3: DEVELOPMENT CONCEPTS

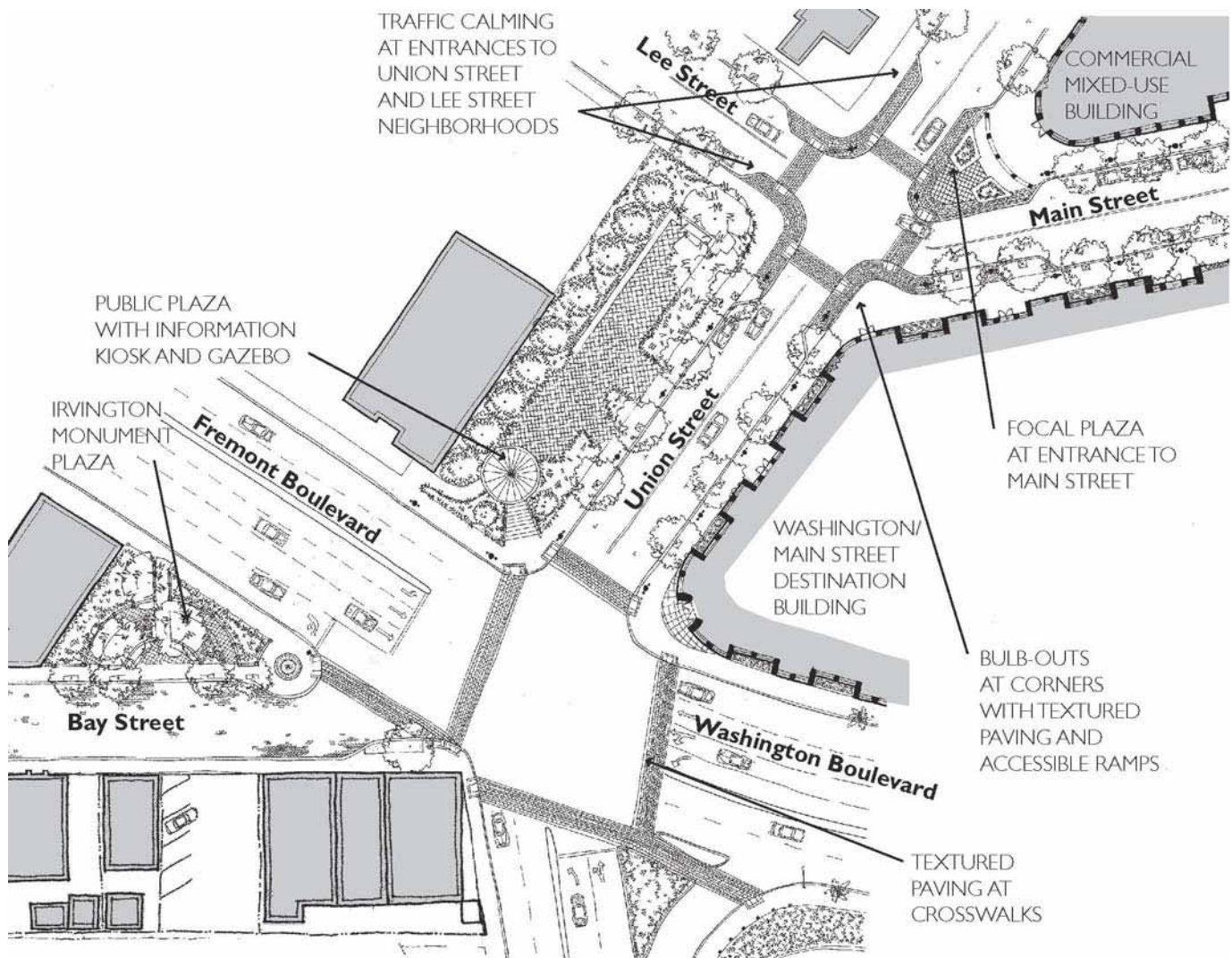


Figure 3-10. Concept Plan Vision for Five Corners

2. Parking

The parking strategy for the Washington/Main Street Triangle includes on-street parking on Main Street in addition to a parking lot and a multi-level parking structure, both in the center of the block. Access to the parking facilities should be via Main Street and Roberts Avenue in order to provide maximum street frontage for commercial development and minimal curb cuts across sidewalks.

3. Circulation and Traffic-Calming

Main Street can become an important mixed-use spine, and may also one day serve as an access to a BART station via an undercrossing of Washington Boulevard. Thus this focus area must accommodate through-traffic while also providing adequate pedestrian amenities. Street improvements should discourage non-local traffic from entering the adjacent neighborhoods via Union Street, north of Main Street, and Lincoln Street and Lee Street, west of Union Street. Figure 3-10 illustrates curb bulb-outs and street trees at these access points. Other traffic-calming features should be explored as development occurs. These elements can include both alterations at the access points to the neighborhood and changes in the neighborhood's interior street features that discourage through-traffic between Five Corners and Grimmer Boulevard. Such changes could include stop signs, mandatory vehicular turns or even closing a street to vehicular through-traffic.

C. Main Street

The existing land use pattern on Main Street includes a mixture of automobile-oriented commercial uses and single-family residences. The Washington Boulevard Grade Separation Project and the potential Irvington BART station will have a significant impact on the future development of Main Street. The grade separation project will include sufficient right-of-way underneath the bridge to allow Main Street to connect to development south of Washington Boulevard, including the potential BART station.

1. Overall Concept

As noted above, development on Main Street should create a mixed-use spine on the east side of the Five Corners, while also taking advantage of the close proximity to the potential BART station. New development should be comprised of ground-floor commercial uses with office and/or residential uses above. Ground floor commercial spaces could also be live/work units, depending on market conditions. Street trees, bulbouts at street corners and other pedestrian amenities will define Main Street, as illustrated in Figures 3-11, 3-12 and 3-13. Buildings should be two-to-three stories tall and built to the back of the sidewalk in order to define the street and pedestrian environment.

CHAPTER 3: DEVELOPMENT CONCEPTS

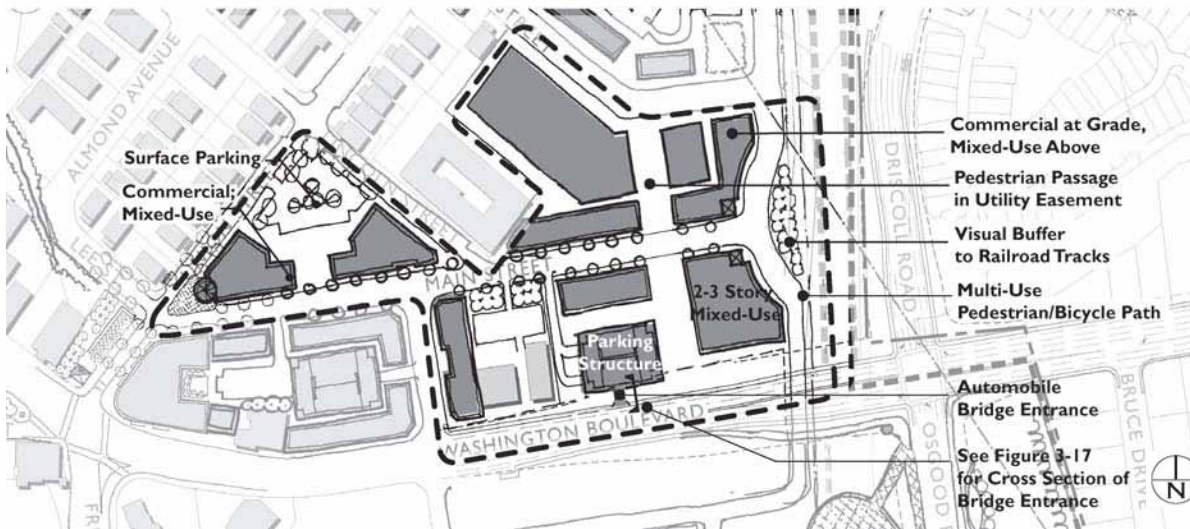


Figure 3-11. Main Street Development Concept

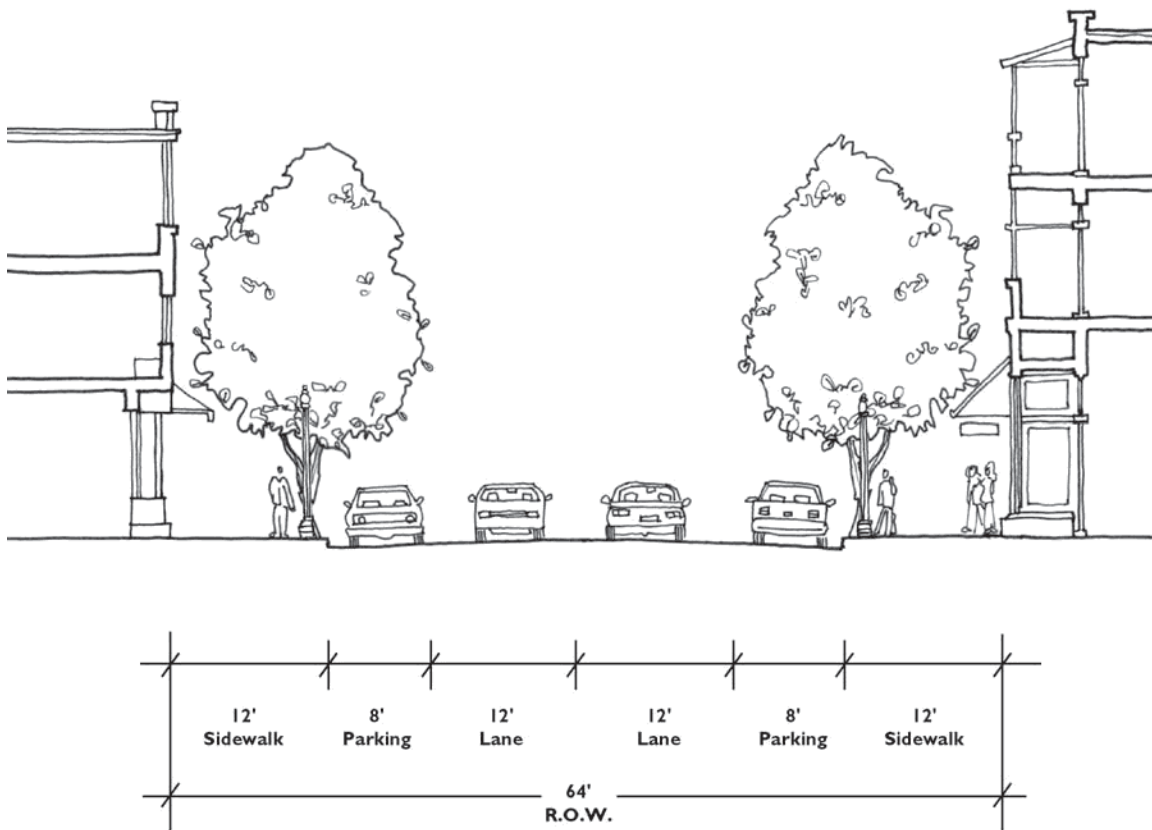


Figure 3-12. Main Street Section

CHAPTER 3: DEVELOPMENT CONCEPTS

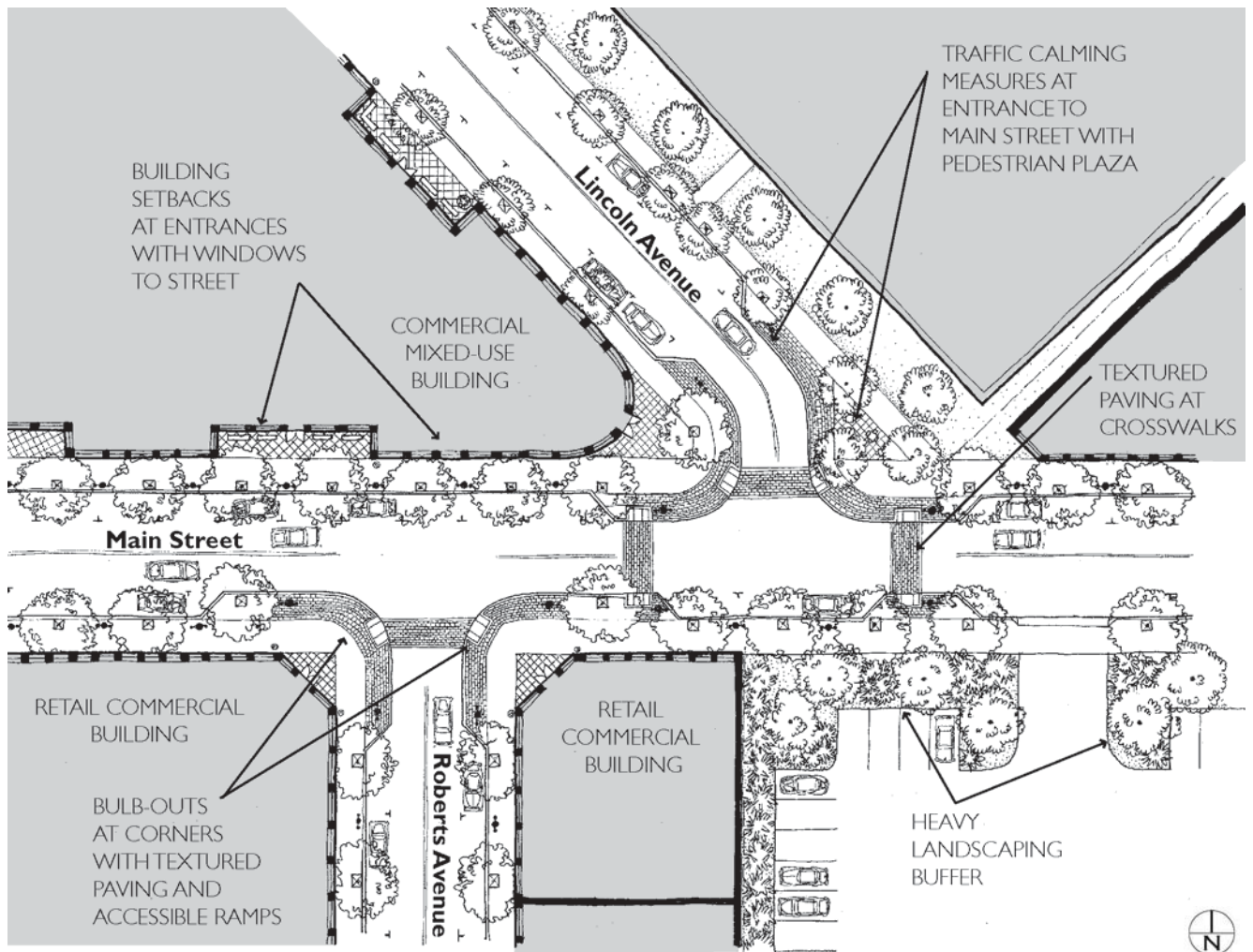


Figure 3-13. Long-Term Vision for Main Street Intersection at Roberts Avenue and Lincoln Avenue



Figure 3-14. A view on Main Street



Figure 3-15. A view of the Main Street Terminus

Main Street will end at the east with a green space and plaza or a traffic circle. This element will buffer development from the rail tracks and terminate the view down Main Street. The terminus of Main Street will also facilitate connections to and from the future BART station entrance south of Washington Boulevard. Buildings at the east end of Main Street will define the plaza and green space at the Main Street terminus. A conceptual view of this area is shown in Figure 3-15. Illustrated in Figure 3-16 are landscaping treatments, ground-floor commercial entrances onto the plaza and the multi-use pedestrian/bicycle path.

2. Parking

The streets in this focus area will be designed to include on-street parking, as illustrated in the sectional view of Main Street in Figure 3-12. The streets, particularly Main Street will contain pedestrian-friendly design elements, such as curb bulb-outs, street trees and furniture so as to facilitate a safe and walkable connection to the BART station.

Off-street parking will include a combination of surface lots and structured parking. The parking facilities should be designed so as to maximize the street frontages in this focus area with active uses.

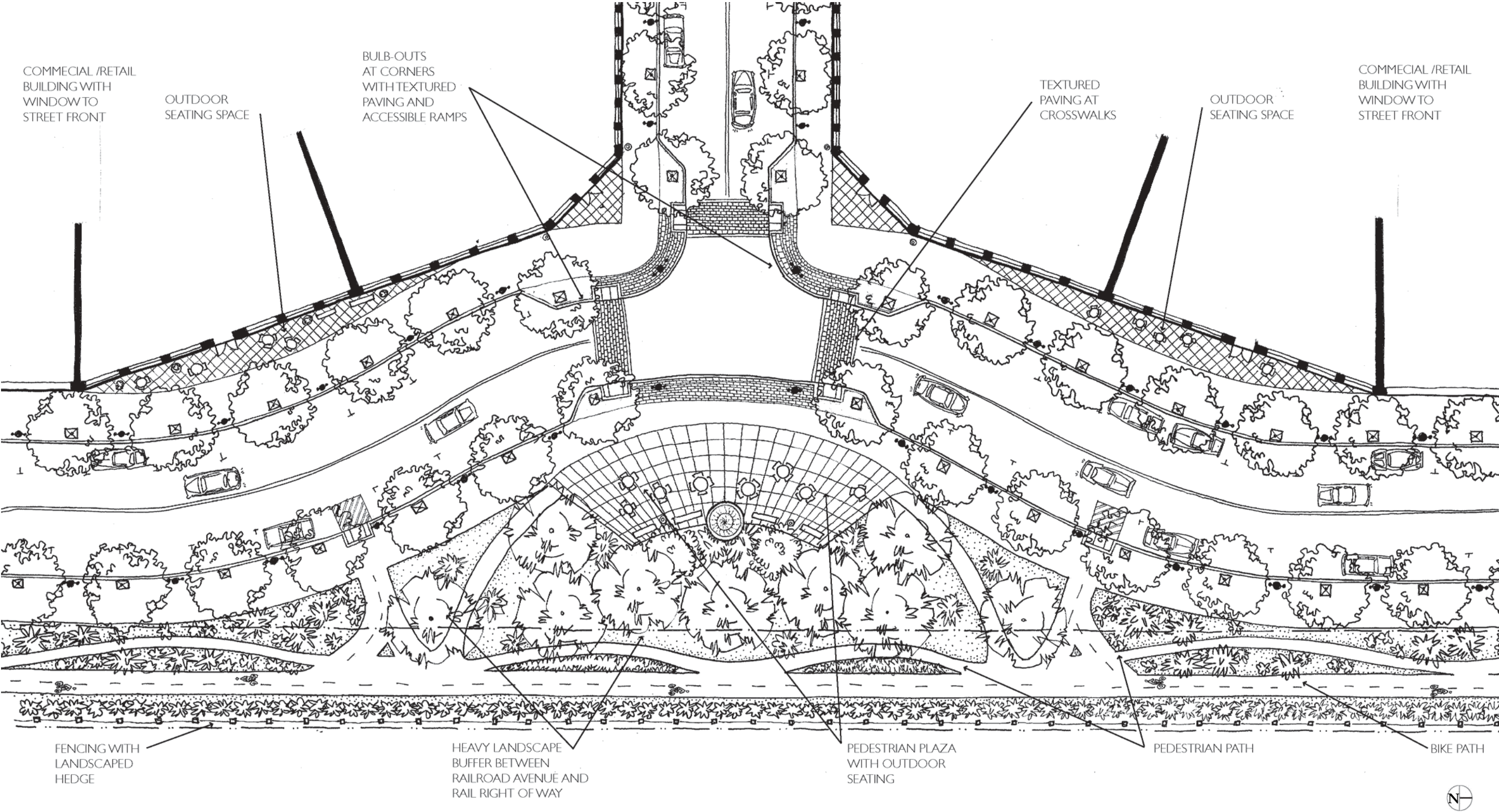


Figure 3-16. Main Street Terminus at Railroad Tracks

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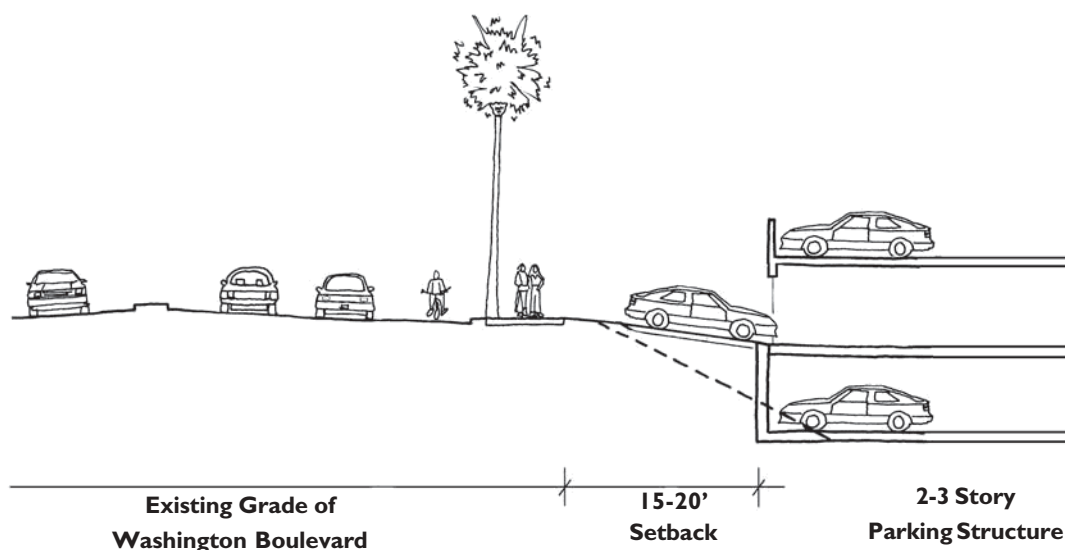


Figure 3-17. Washington Boulevard Bridge and Parking Garage

In particular, the elevated grade of the Washington Boulevard overpass will provide an opportunity for multi-level parking access to a garage built between Washington Boulevard and Main Street. This garage could help revitalize Main Street by serving both mixed-use development on Main Street and BART patrons, bringing pedestrians and commercial activity to the improved Main Street corridor. Its potential location is shown on Figure 3-11, while Figure 3-17 is a sectional drawing showing its split level access.

3. Circulation and Traffic-Calming

Access to this focus area will occur primarily via Union Street and Roberts Avenue. Main Street will be a destination itself, but it will also ultimately be a gateway to the BART station for Irvington residents. The street network in this

focus area will enable traffic to disperse because many of the streets connect either to Main Street or the new street parallel to the railroad tracks. However, measures should be taken to hinder BART-related traffic from entering the adjacent residential areas enroute to Grimmer Boulevard or Paseo Padre Parkway. Traffic-calming facilities should be considered for the Union Street intersections with Lee Street, Lincoln Street and High Street. Facilities at these locations could include curb bulb-outs, textured paving or speed tables. Other tools could be investigated that alter through-circulation patterns in the interior of this residential neighborhood.

Adjacent to the new street under Washington Boulevard will be a multi-use pedestrian and bicycle path that extends from the BART station to Central Park, north of Paseo Padre Parkway.



Figure 3-18. Irvington BART Station Development Concept

D. BART Station Site

The City and BART are continuing to investigate the placement of a new BART station in Irvington immediately south of the intersection of Washington Boulevard and Osgood Road. The station is currently unfunded, but is included in the BART Warm Springs Extension Environmental Impact Report (EIR). BART consultants have completed a preliminary concep-

tual design for the station based on conditions on Osgood Road that will result from the pending Washington Boulevard grade separation. In addition, the City will be funding a Constructability Study to ensure that design of the Warm Springs Extension includes those features needed to accommodate the future Irvington Station.

Development constraints for this focus area include the historic Gallegos Winery site and ruins, the Hayward Fault alignment and the proximity of established residential neighborhoods and streets.

1. Overall Concept

The BART station, with an entrance on the west side, will generate retail, office and residential activities on Main Street. The station should be attractively designed to be a local landmark and community asset. The station site has two primary access points.

- ◆ Irvington community members will tend to access the station from Main Street and the connection under Washington Boulevard.
- ◆ Regional patrons arriving by vehicle will generally access the station from Osgood Road.

Orienting a station entry toward Main Street will help to facilitate Main Street's role as the pedestrian-oriented commercial link between Five Corners and the BART station.

The Main Street entrance area will also serve as one end of the multi-use pedestrian and bicycle path between the Irvington BART station and Central Park. The path will travel under the Washington Boulevard overpass and along the railroad before crossing over Paseo Padre Parkway and connecting to the park.

2. Parking

The primary parking facility for the Irvington BART station will be located east of Osgood Road, with station access provided by a pedestrian bridge over Osgood Road. Preliminary conceptual plans place handicap parking and a bus transfer area between the station and Osgood Road. Other parking opportunities might exist immediately west of the station with access provided by the connection to Main Street. As shown in Figure 3-18, one concept is to construct a parking structure that can be accessed from the Washington Boulevard grade separation and Main Street, such a parking facility would serve both BART and local businesses on Main Street.

E. Railroad Parcels

Realignment of the Union Pacific Railroad tracks to the east as part of the Washington Boulevard Grade Separation will create new opportunities for the existing industrial parcels. Instead of being separated from Irvington by the railroad tracks, these parcels may abut existing residential uses as well as the newly reconfigured Main Street area. A large retail or commercial development would not be appropriate in this area due to limited access and the existing residential neighborhood to the west. The Hayward Fault runs across the southern part of this area.

1. Overall Concept

This focus area will be developed with residential uses in order to take advantage of its close proximity to the BART station. A new north-south loop road will run the length to serve this area, as there will be no direct access to Paseo Padre Parkway. The multi-use pedestrian and bicycle path connecting the BART station and Central Park will run through this focus area and connect over Paseo Padre Parkway with a pedestrian/bicycle bridge.

Development plans for the railroad track realignment and related realignment of telecommunications, electrical and other infrastructure components are currently underway.

In the scenario shown in Figure 3-19, proposed residential densities would be highest at the southern end near the Main Street terminus and would decrease as development moves north, away from the station. Densities would range from 27 to 50 dwelling units per acre further south (near the future BART station), to 8.3 to 23 dwelling units per acre at the north. All new development should be sensitive to existing adjacent residences.

Surface parking lots should take advantage of the fault line easement, where building structures are not allowed. This is especially true of the triangular block between Union Street, High Street and the new street along the railroad property. On-street parking should also be provided for all residential parking areas with public streets.

Multi-family development should orient building entries to the street and locate automobile access at the rear of the property on surface lots or tucked under buildings.

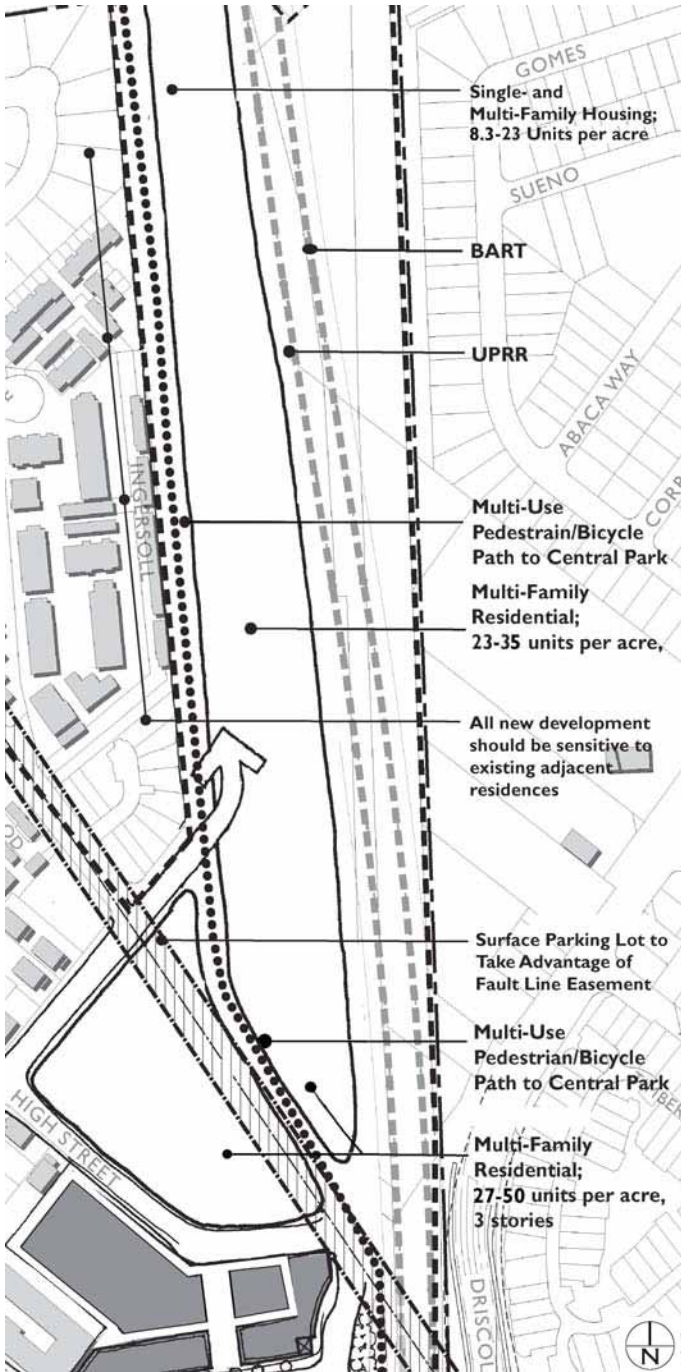


Figure 3-19. Railroad Parcels
Development Concept

2. Circulation and Traffic-Calming

Limited curb cuts on the street should provide access to grouped parking areas, minimizing conflicts with pedestrians. This is particularly important in this residential area so that pedestrian access to the possible future BART station would be convenient and safe.

Primary access to this area will occur from Union Street. Secondary access will occur from High Street. In order to reduce the effects of increased traffic in adjacent neighborhoods, the installation of traffic-calming devices should be considered in conjunction with development in this focus area. Fire and emergency installation of traffic-calming devices should be considered in conjunction with development in this focus area. Fire and emergency access through existing neighborhoods should be explored.

F. Grimmer South

The Grimmer South focus area includes large parcels with vacant or underutilized commercial uses. Opportunities exist to redevelop some of these parcels and their accompanying parking areas. Bay Street intersects with Grimmer Boulevard in this area.

1. Overall Concept

New development in this focus area will be primarily multi-family residential. Development should have active street fronts to facilitate safe and walkable environments. Corner buildings on Grimmer Boulevard and Bay Street will create a strong entry feature linking Grimmer Boulevard to Five Corners via Bay Street. Development along Fremont Boulevard in this focus area should be primarily commercial.

New commercial and mixed-use development in this focus area should occur primarily at the street edge, with buildings facing Grimmer and Fremont Boulevard and parking located in the middle of the development behind buildings, rather than adjacent to the street. Substantial opportunity exists for residential development in the interior of these large parcels. This residential development is envisioned to be medium and high density and would be compatible with existing adjacent multi-family residential projects. Access and parking needs should be evaluated in detail as part of a site-specific traffic study when a project is proposed.

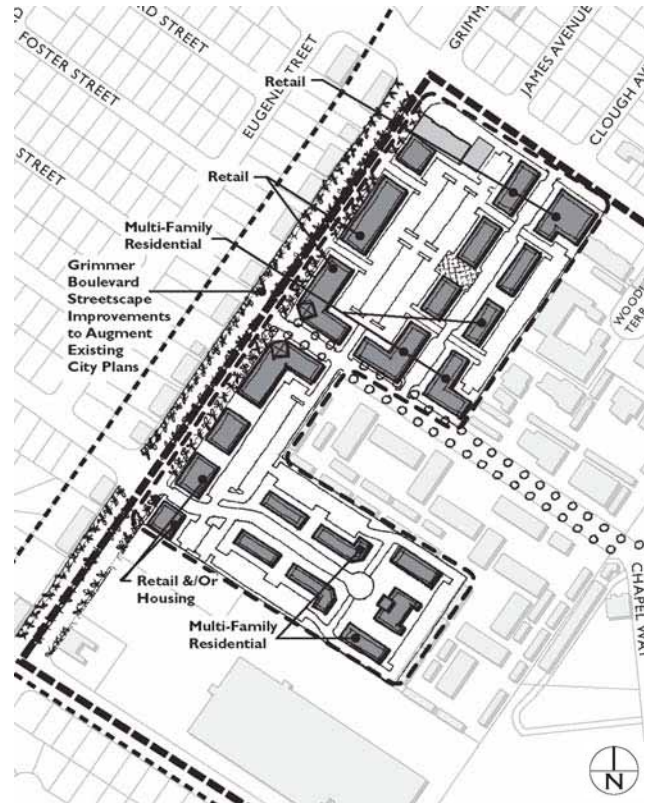


Figure 3-20. Grimmer South Development Concept

A beautification program for Grimmer Boulevard south of Fremont Boulevard would tie into a planned street program adjacent to the creek channel to the north of Fremont Boulevard. An improved Grimmer Boulevard streetscape will enhance the redevelopment opportunities for properties along the west side of Grimmer Boulevard, which was not studied as part of this Concept Plan.

G. Fremont Boulevard

Several parcels that are vacant or contain businesses in converted residences are located along Fremont Boulevard, south of Washington Boulevard. Recently, a number of multi-family projects have been built along this corridor, which is the primary vehicular entry to Irvington and Five Corners from the south. New development along this corridor can provide greater continuity to this strategic entry to Irvington.

1. Overall Concept

Development in this focus area should reinforce Fremont Boulevard's role as the primary southern entrance to Irvington at Five Corners. New development north of Adams Avenue, including the intersection of Irvington Avenue and Fremont Boulevard, should have commercial components enhancing the commercial activity extending south along Fremont Boulevard from Five Corners.

Development along Fremont Boulevard south of Adams Avenue should be multi-family residential with appropriate setbacks. Development should be two to three stories with adequate on-site parking placed at the rear of parcels. Driveway access should be provided from side streets where feasible to minimize the number of curb cuts on Fremont Boulevard. This will allow for continuous sidewalks and maximize building frontages along this important corridor and entry to Irvington.

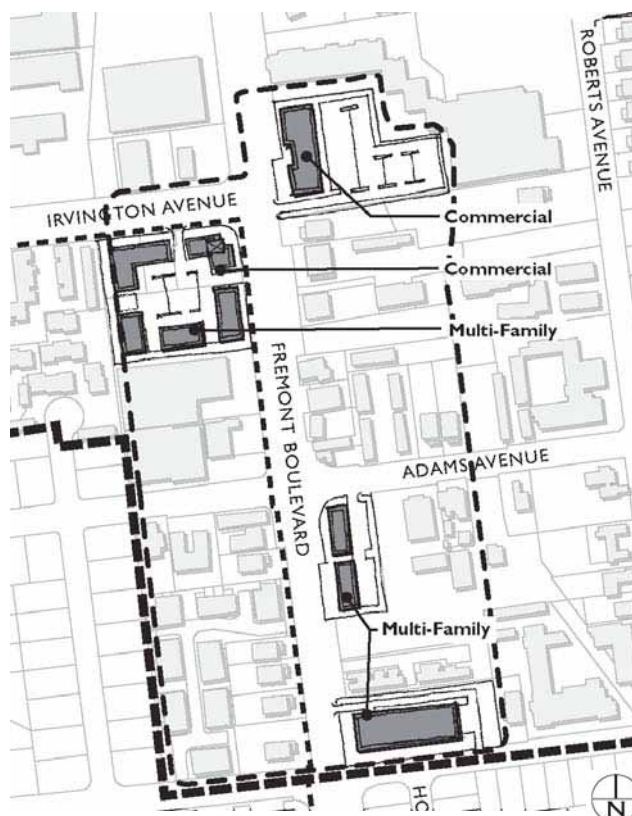


Figure 3-21. Fremont Boulevard Development Concept

H. Creeks

Open space or small passive recreation opportunities should be considered along those parts of Laguna Creek that are not constrained by privately owned parcels. The system of creeks that passes through Irvington, particularly those segments that have not been channeled underground by development, are a natural feature unique to Irvington and its history. Those segments that are still above ground can inform future development in Irvington similarly to the ways that the older historic buildings will.



Figure 3-22. Laguna Creek in Irvington

As recommended in the City's adopted *Laguna Creek Basin Reconnaissance Study*, development occurring adjacent to creeks should explore, to the extent feasible, opportunities for creek and channel enhancements. Areas or portions of the creek near public streets or civic uses could be restored and landscaped as open space with educational components. The primary opportunities for creek restoration in Irvington are denoted with an "H" in Figure 3-2.

I. Concept Plan Illustrative

Figure 3-23 combines the development concepts for each of the Concept Plan focus areas to illustrate the potential for Irvington's built environment with implementation of the conceptual plans and improvements described in the Irvington Concept Plan.

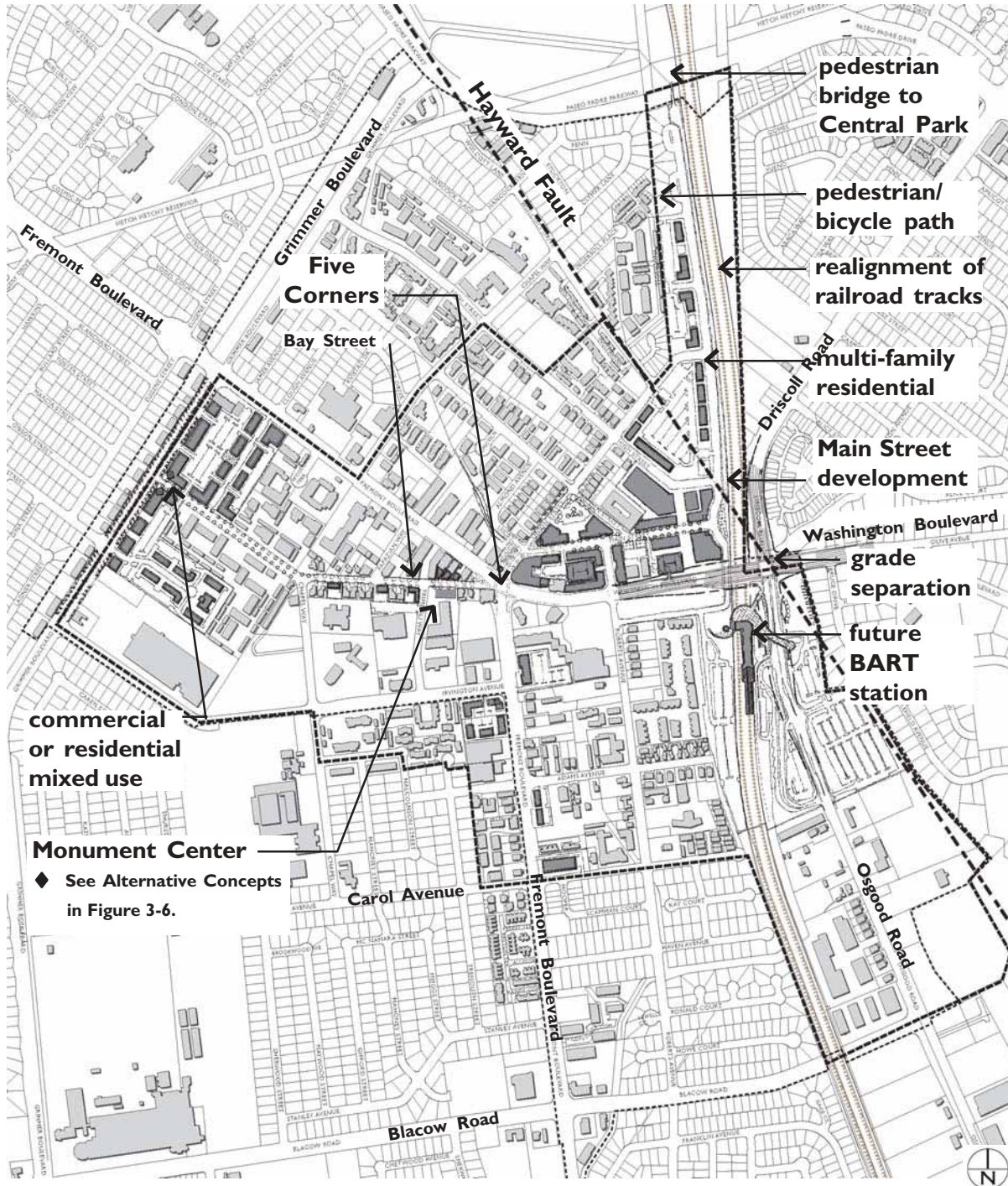


Figure 3-23. Development Concept Preferred Illustrative

CHAPTER 3: DEVELOPMENT CONCEPTS

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4 DESIGN GUIDELINES

This chapter contains the building design guidelines for new development in the Irvington Concept Plan Area, which are intended to achieve the vision and goals of the Concept Plan presented in Chapter 2. Adoption of the Concept Plan will include adoption of the Design Guidelines that follow. These guidelines will supersede the Irvington Design Guidelines previously adopted on July 27, 1993.

A. *Definitions*

The Design Guidelines in this chapter contain language that relates only to the Concept Plan Area. Further refined design guidelines exist for Bay Street and will apply only to Bay Street. The language in this chapter reflects the following principles:

- ◆ **“Shall” or “Must”** means that conformance to the guidelines is intended to be mandatory.
- ◆ **“Should,” “May” or “Encouraged”** means that conformance to the guideline is preferred, that conformance will be strongly encouraged by the City through the review process and that the guideline is intended to be a recommendation to a developer about how to meet the goals of the Concept Plan. However, conformance with these guidelines is not mandatory, and alternatives may be suggested by developers and property owners.

B. *City of Fremont Project Review Process*

Commercial Projects in areas zoned with the Irvington (I) Overlay are subject to review by the Development Organization, the Planning Commission, and/or the City Council. The Historical Architectural Review Board (HARB) also reviews some projects as described below. The nature of the project determines the appropriate reviewing agency or agencies. The Irvington Design Guidelines herein will be used in addition to the Zoning Ordinance standards in evaluating projects in the overlay district.

1. **Development Organization**

The Development Organization, is composed of City staff, and reviews all new commercial construction, multi-family residential construction of ten or less units and all exterior modifications to existing commercial buildings.

2. **Historical Architectural Review Board**

The Historical Architectural Review Board (HARB) reviews project proposals for any exterior changes or new construction for all properties within an Historic Overlay District (with the exception of single family dwellings) and all Primary Historic Resources. A list of these Primary Historic Resources in Irvington is included at the end of this chapter.

3. Planning Commission/City Council

Projects required to be reviewed by the Planning Commission and/or City Council, such as Conditional Use Permits and Planned Districts, will be assessed for conformance with the Guidelines by staff prior to consideration by the Planning Commission or City Council.

C. *Design Guideline Categories*

Design Guidelines will only apply to certain parcels in the Irvington area. The applicability of the Design Guidelines depends on the type of street that a parcel faces, as shown in Figure 4-1:

- ◆ **Bay Street.** Development facing Bay Street has its own Design Guidelines, entitled the “Bay Street Planned District Design Guidelines,” which are not included in this document and can be obtained separately from the City of Fremont. Properties located within the Bay Street Planned District must conform to these guidelines. Any properties facing Fremont Boulevard should first follow the Major Street design guidelines in this chapter. The Bay Street Planned District Design Guidelines will further refine the Major Streets design guidelines.

- ◆ **Pedestrian-Oriented Streets.** Development on these streets should facilitate a safe and comfortable pedestrian environment for residents and patrons of commercial establishments. Design Guidelines for development facing these streets are included in Section D (pages 52 to 70) of this chapter.

- ◆ **Major Streets.** Development on these streets should provide safe and aesthetically pleasing street frontages although these streets will continue to be the primary vehicular corridors in and through Irvington. Design Guidelines for development facing these streets are included in Section E (pages 71 to 81) of this chapter.

- ◆ **Parcels facing onto all other streets** do not need to comply with specific Design Guidelines.

In cases where a parcel has frontages on more than one street, each frontage should conform to the standards and guidelines for its respective street type.

CHAPTER 4: DESIGN GUIDELINES

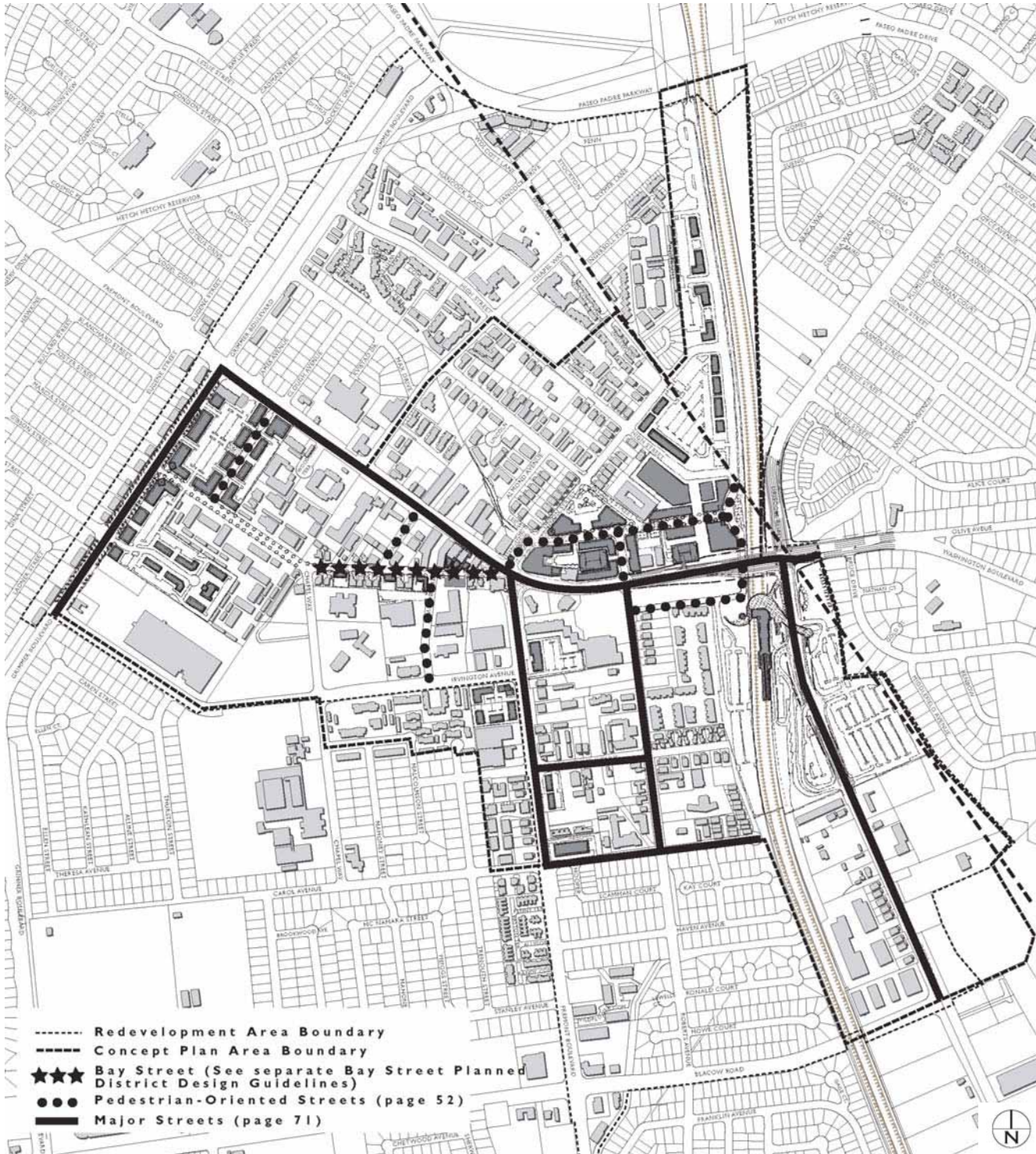


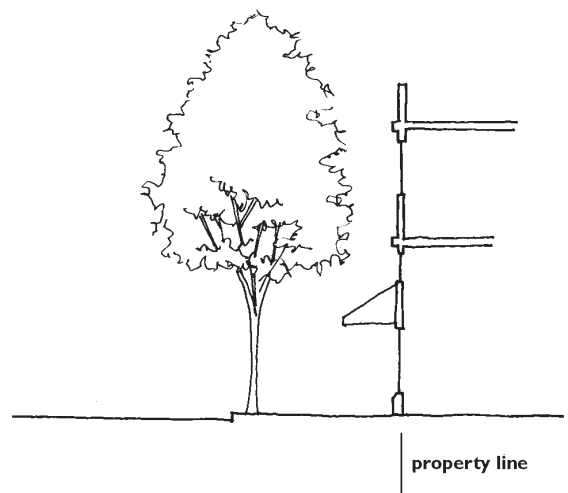
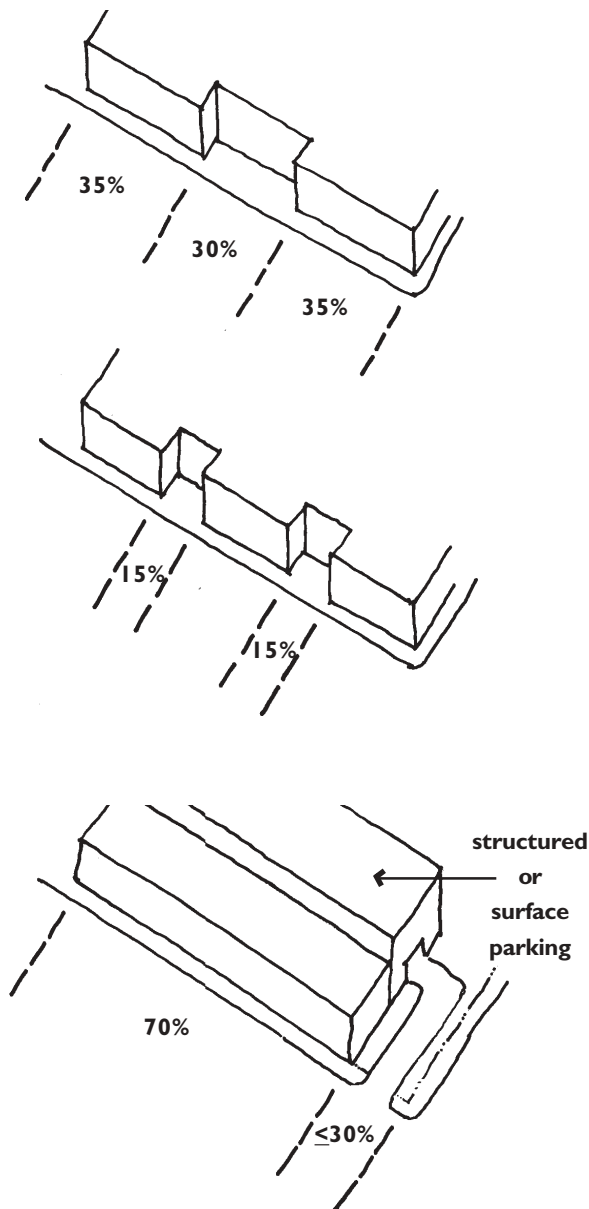
Figure 4-1. Street Categories for Design Guidelines

D. Pedestrian-Oriented Streets

The following building design guidelines apply to new development facing pedestrian-oriented streets.

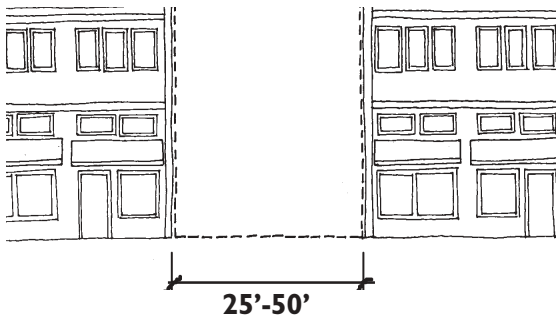
1. Building Setback

At least 70 percent of each front property line shall be lined with a building. Up to a maximum of 30 percent of a front property line may be setback, but only as a means to create plazas or open spaces or to access parking facilities behind the building. An exception may be made for parking structures, as detailed on page 68, and for narrow lots.



2. Building Rhythm and Massing

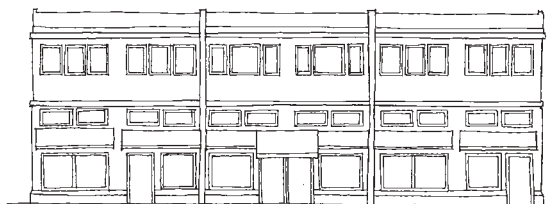
Buildings **shall** be articulated to reflect a small-scale street frontage rhythm, with building storefront widths of approximately 25 to 50 feet. Where appropriate, bay width and massing **should** reference adjacent commercial buildings.



3. Building Façades

a. Articulation

Buildings **should** have architecturally-articulated storefronts. Window treatments, awnings and public entries **should** be designed to promote active use of ground floor businesses.

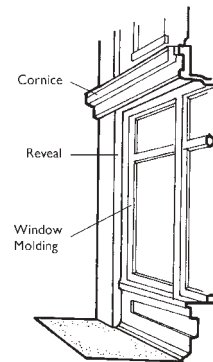


CHAPTER 4: DESIGN GUIDELINES



b. Scale of Detailing

Building facades **should** have elements that relate to the scale of a person. All facades **shall** emphasize three dimensional detailing such as cornices, window moldings and reveals to cast shadows and create visual interest on the facade. Architectural elements used to provide relief **may** include awnings and projections, trellises, detailed parapets and arcades. The historical elements depicted below are an illustrative example only and are not binding as to style.

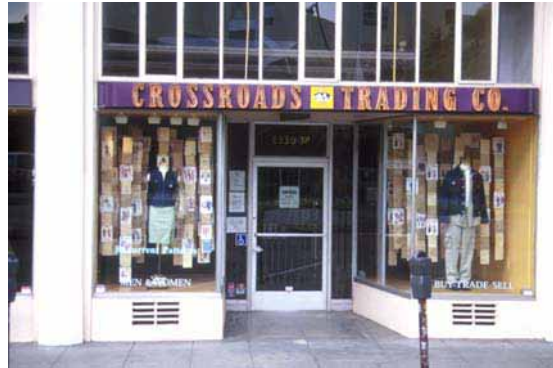


c. Roof Lines

All buildings **shall** provide strong roof termination features. A variety of distinctive roofline profiles is **encouraged**.

d. Entries to Ground Floor Areas

Entries to ground floor retail areas **shall** occur from main streets, and **shall** be accented with features such as moldings, lighting, overhangs, or awnings. Building entries **should** be recessed into entry bays, to create transitional spaces between the street and buildings. Corner entries **should** receive special attention.



e. Entries to Upper Levels

Street level entries to upper level commercial or residential uses **should** be emphasized if they are located on the front facade.

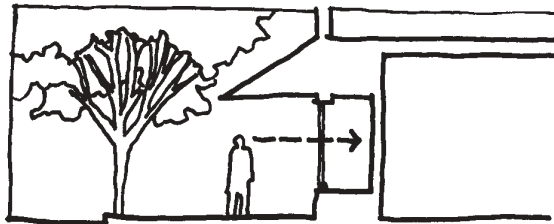
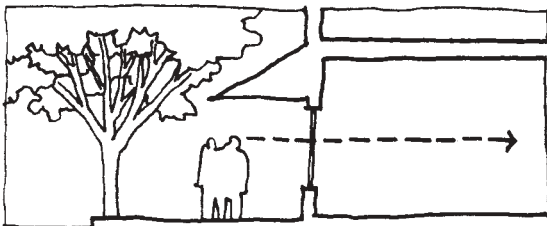
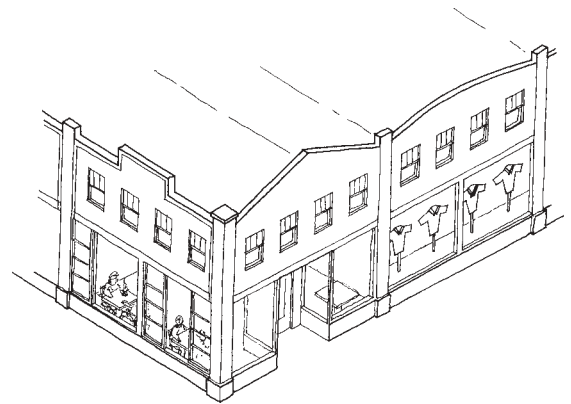


CHAPTER 4: DESIGN GUIDELINES



f. Windows

- i. Window patterns **should** architecturally distinguish a building's first floor retail character, with a higher percentage of windows than on upper floors.



- ii. Commercial storefronts **should** include street-oriented display windows. These windows **should** provide visual access to the inside of the building, while also serving as an area for merchandise display. A minimum of 50 percent of linear store frontage **should** be used for the display windows.

- iii. Enclosed display window areas **should** be provided where actual windows cannot be provided.

iv. No windows are required for buildings with unique requirements, such as theaters or parking structures. In such cases, exterior walls **shall** be designed to provide architectural relief or are screened by landscaping and pedestrian amenities such as wider sidewalks or benches.



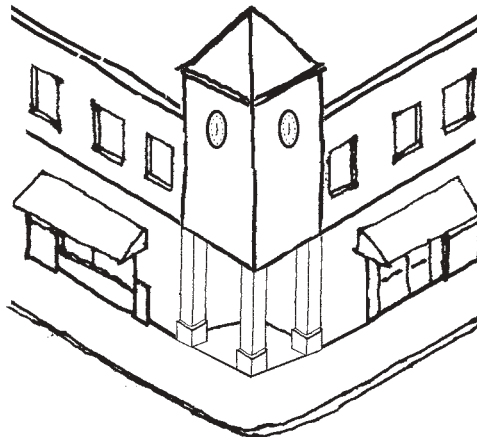
v. Windows **should** be of clear glass unless unique operating circumstances prohibit it. Decorative glass and glass block are also appropriate in many cases.

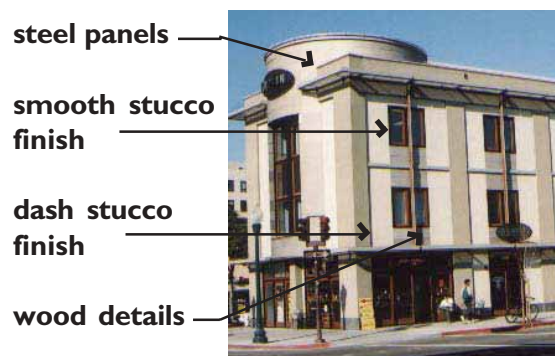
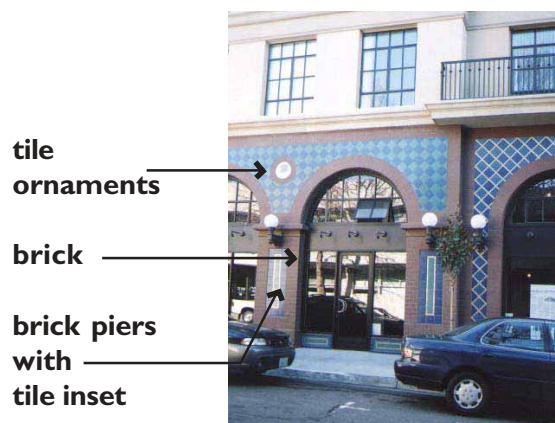
g. Bays

Bays on buildings are allowed and **encouraged**. If bays are used, they **may** protrude out from the building.

4. Corners

Building corners are special places within the townscape. They **should** therefore be treated specially, with elements such as towers or indented building entries.





Examples of building materials.

5. Building Materials

a. Variety

A variety of durable materials and textures is **encouraged** in Irvington. Such materials **may** include both traditional materials such as wood and stucco and materials such as concrete, structural steel, corten steel, and other high-quality durable metals which have not been traditionally used in “Main Street” architecture.

b. Differentiation of Architectural Elements

A wide variety of other materials to articulate different building elements beyond the main exterior finish such as the building base, horizontal break bands, pier/column bases, roof terminations, sills, awnings, etc. is **encouraged**.

c. Reference to Adjacent Buildings

As Irvington continues to develop, designers **should** reference adjacent commercial buildings in order to complement the existing materials palette.

d. Building Bases

Building bases **should** be of durable materials. Stucco-covered foam at the sidewalk is discouraged due to high pedestrian traffic.

e. Decorative Elements

i. Tile artwork, plaques, decorative glass and lighting fixtures are **encouraged** to provide visual relief to facades.



ii. Artwork and architectural relief integrated into buildings is **encouraged** and **should** be appropriate to the setting.

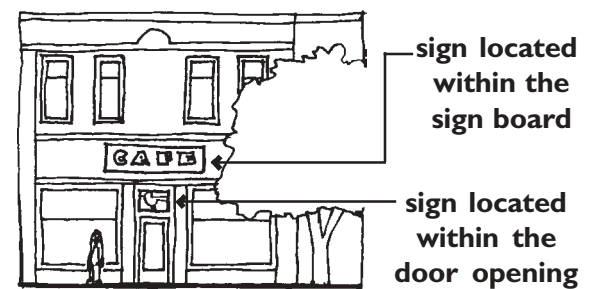




6. Signs

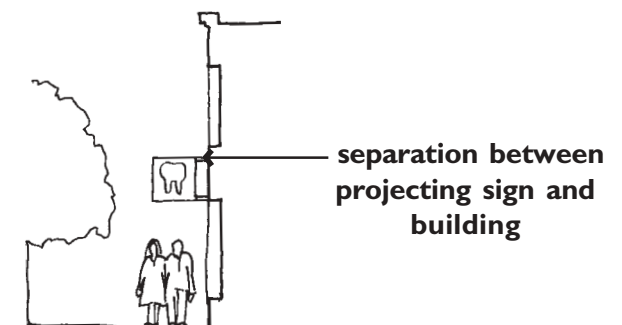
a. Sign Location

Building signs **should** be located within an area of the facade which enhances and complements the architectural design. Buildings signs **should** not obscure architectural details such as recesses, ornaments or structural bays. Building signs **should** not extend above the roof line of the building.



b. Projecting Signs

Projecting signs **should** be located near the front entry of a store. A separation **should** be provided between the building face and the sign in order to provide a clear separation between the building edge and signage.



7. Awnings

a. Relationships to Building Design and Bays

Where proposed, individual awnings **should** generally be provided over each storefront on a building. These awnings **should** be located within the individual structural bays and **should** not hide architectural detailing. Awnings on multi-tenant buildings **should** be the same color and style.



Example of awnings within structural bays.



CHAPTER 4: DESIGN GUIDELINES



b. Awning Materials

The use of fabric awnings is **encouraged**, although glass and metal awnings may also be used where appropriate. The use of vinyl awnings is discouraged.



c. Awning Signage

Any signing on awnings **shall** be painted directly onto the awning material. Awning signs **shall** be restricted to the lower one-third of the awning and the awning valance.

d. Illuminated Awnings

Backlighting of transparent or translucent awnings is **discouraged**. Exceptions may be made if done aesthetically or if such lighting is related to a use, such as entertainment.

e. Awning Lighting

If used, lighting for awnings **should** be from above the awning, from fixtures designed and placed to enhance the appearance of the building.

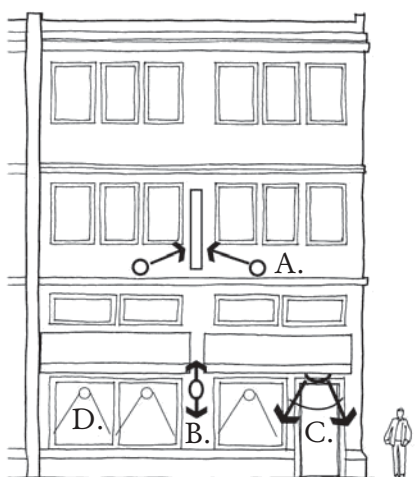
f. Awning Colors

Awning color **should** be compatible with the neighborhood and overall building color scheme.



8. Lighting

Adequate lighting **should** be provided for building signage, storefront display, pedestrian entry access and travel in parking lots.



A. Directional Sign Illumination

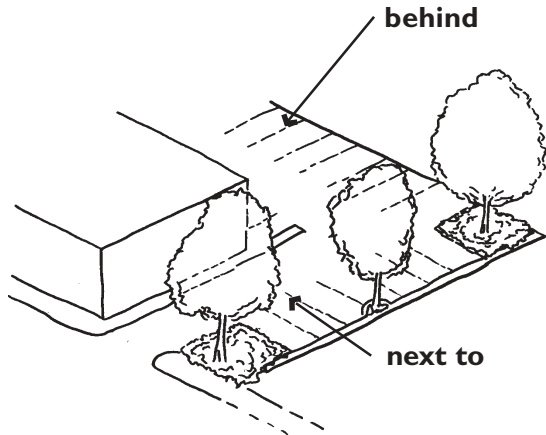
B. Wall Sconce

C. Wall Downlight or Recessed Light

D. Interior Display Light



Types of lighting sources for illumination of facades and display windows.



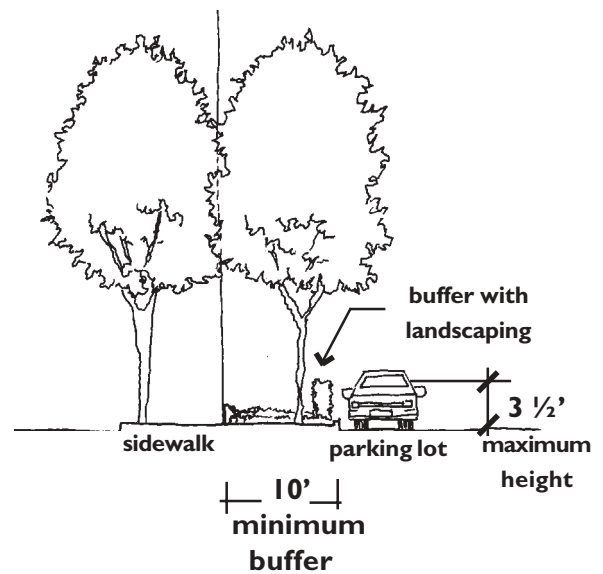
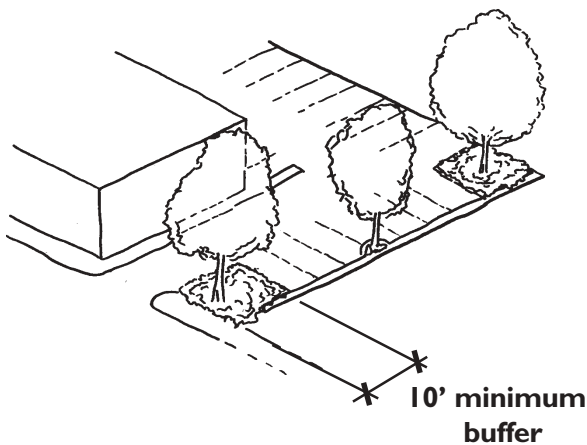
9. Parking Lots

a. Location

Parking lots **shall** be located behind or next to buildings, whether in parking structures or surface lots.

b. Landscape Buffer

At access points to off-street parking lots, a landscape buffer shall be provided and shall be a minimum of ten feet deep. Most plants in the buffer **should** be no higher than three-and-one-half feet in order to maintain maximum sight distances, although occasional trees are allowed. Where lot width allows, a landscape buffer **shall** be provided at property lines of adjacent parcels.



c. Landscape Aesthetics

i. Trees **should** be planted in and around parking lots to provide shade and visual relief. Trees **should** be planted on the perimeter of parking lots at a density to conceal or disguise the interior of the parking lot. On the interior of parking lots, trees **should** be planted to create shade and minimize heat.



ii. The palette of plant materials in the landscape buffer **should** create an aesthetically-pleasing space through a mix of colors, heights and types of plants.

- ◆ Trees **should** provide vertical interest.
- ◆ Shrubs **should** be used to provide a natural fence that shield autos from the pedestrian sidewalk.
- ◆ Flowering plant material at the ground level is **encouraged** to provide a variety of colors.
- ◆ Creative design and the use of native plants and drought tolerant plans are **encouraged**.



CHAPTER 4: DESIGN GUIDELINES



Examples of fixtures that add visual interest.

d. Lighting

Lighting fixtures in parking lots **should** be selected to add visual interest while complementing nearby architecture. They **should** be scaled to pedestrians.



Permeable concrete paver units add texture.

e. Parking Pavement Surfaces

i. Patterned pavement and integral colored concrete are **encouraged**, especially in parking lots that will have multiple uses or be visible to the public.

ii. Permeable surface finishes **should** be used to add surface texture and to facilitate stormwater runoff.

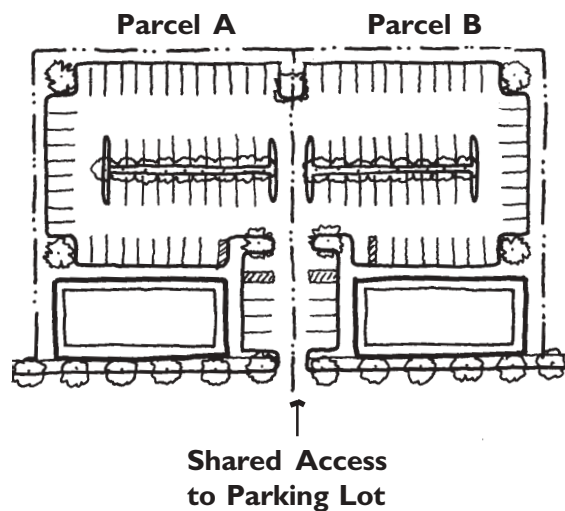
f. Vertical Elements

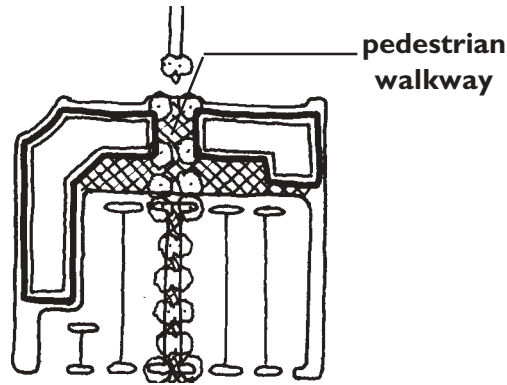
Vertical elements such as screens or trellises **should** be incorporated into parking lots to provide visual interest, both at the parking lot edges and within the parking lot.



g. Shared Access Entries

Building siting and parking design **should** maximize opportunities for pedestrian and vehicular circulation between adjacent sites. For example, joint access ways serving two or more buildings are **encouraged** as a means to minimize curbcuts and avoid breaks in continuity of street frontages.





h. Pedestrian Circulation

Pedestrian access separate from vehicles **should** be provided from all public sidewalks and parking lots to building entrances. Pedestrian walkways and spaces **should** include elements such as special paving materials, raised curbs, trellis structures, landscaping, pedestrian-scaled lighting, seating and trash receptacles.

10. Parking Structures

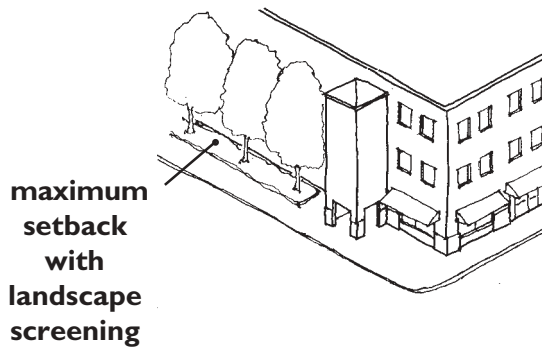
a. Ground Floor Use

As a means to provide street interest, ground floor retail uses **should** be integrated into parking structures wherever possible.

b. Setbacks

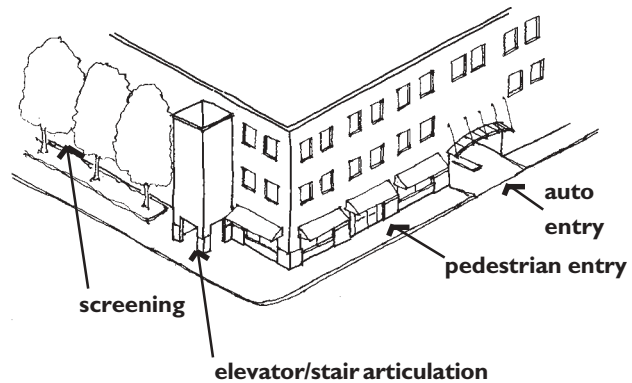
i. As long as parking structures include ground-floor retail spaces, they **should** follow the overall building setback guidelines on page 54.

ii. Where no ground-floor space is possible, parking structures **may** be setback from the property line, as long as the building rhythm is maintained. The setback area **shall** be planted with landscape screening for the structure.



c. Articulation of Facade

- i. Facades of parking structures **should** be designed in a manner that is comparable to other multi-story buildings on the street.
- ii. Openings in the facade **should** be proportioned similarly to vertically-oriented windows.
- iii. Elevator and stair elements **should** be located adjacent to the street and **should** facilitate an architectural articulation of the facade.



11. Private Plazas and Open Space

Many developed sites in Irvington may include publicly-accessible private plazas and open space. These design guidelines reflect direction on the development and use of those spaces.

a. Access

Plazas and open spaces **shall** be visibly accessible at all times, even when the adjacent business is closed. Design concepts **should** be used to discourage a transient population from inhabiting these accessible locations.

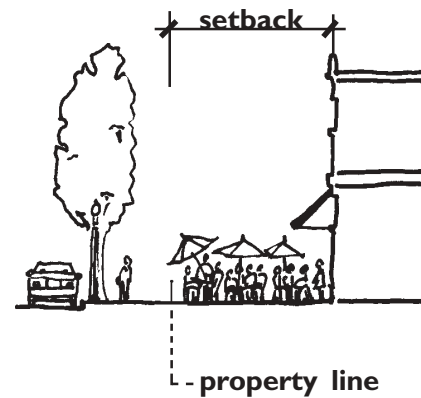


CHAPTER 4: DESIGN GUIDELINES



b. Street Edge

Private open spaces located along public streets in conformance with Guideline 1 (Building Setbacks), **should** accommodate outdoor activities in order to create a lively, vital environment.



c. Required Fencing

Fencing, or barriers required by code to separate dining areas from sidewalks, **should** be semi-transparent and be architecturally compatible with the building.

d. Landscape Materials

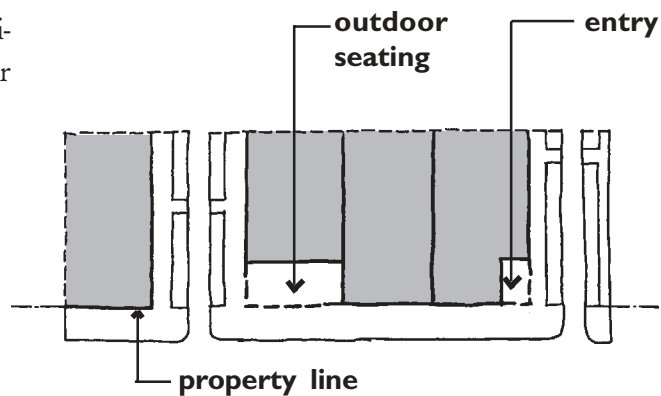
Paving materials **shall** be of high-quality materials such as stone, concrete or tile. Plant materials **should** be of drought-tolerant species where appropriate and provide variety while being consistent with the architecture.

E. Major Streets

The following building design guidelines apply to new development facing arterial streets, shown in Figure 4-1.

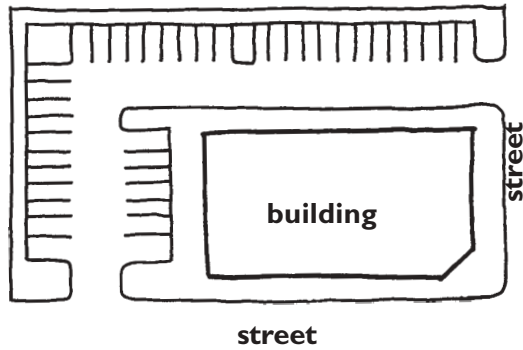
1. Building Locations

a. Buildings **should** be located at the front property line or immediately behind a private or semi-private space, such as an outdoor seating area for a restaurant.



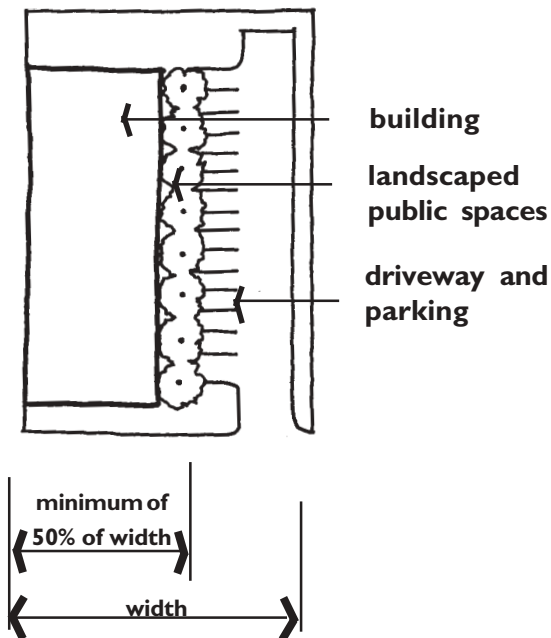
b. Buildings on sites adjacent to public plazas **should** be sited immediately adjacent to the plaza, with openings and doors facing the plaza.





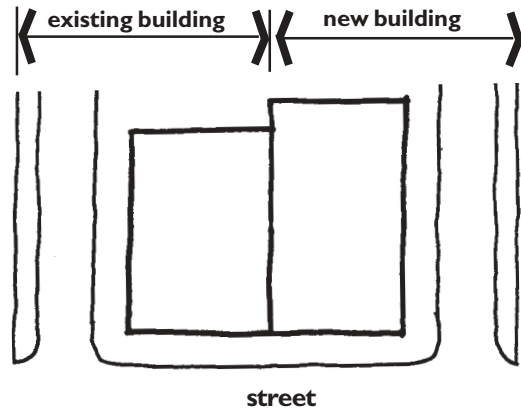
- c. The street corners of corner sites **should** be developed with buildings and not with surface parking. The building **should** either be sited on the corner property lines, or set back from the corner to provide a public open space. Parking **should** be provided behind the building.

2. Building Frontage



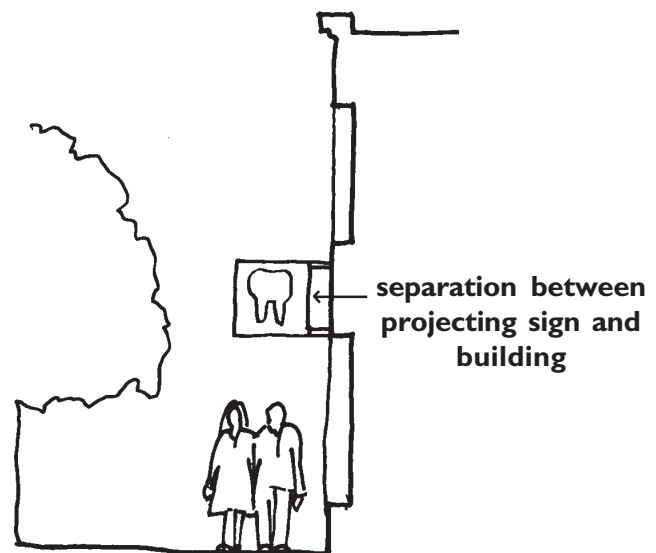
- a. Generally, a minimum of 50 percent of the site's street frontage **should** be developed with a building and/or a landscaped space, unless the width of the site precludes this building width.

b. When a building on one parcel is sited on the front and side property lines, new or expanded buildings on an adjacent parcel **should** also be sited on the front and side property line abutting the neighboring building. The purpose of this guideline is to maximize continuous building frontage at the street.



3. Signs

a. Building mounted signs **should** not extend above the roof line of the building. A separation **should** be provided between the building face and the sign. There **shall** be no interior lighting on projecting signs.



CHAPTER 4: DESIGN GUIDELINES



b. Monument signs, called “low profile, planter-type signs” in the Zoning Ordinance, **should** be designed to be architecturally compatible with the building for which they provide signing. Site addresses **should** be included on the sign. The signs may be illuminated by direct lighting, but shall not be internally illuminated.



c. Other types of freestanding signs, such as pylon signs, are **discouraged**. When used, they **should** be designed and sited to mark the entrance to the site and to harmonize with the building design.

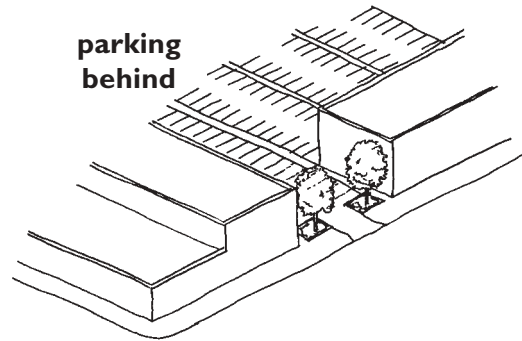


d. Interior-illuminated signs, including individually-mounted letters as well as “can” signs, are not allowed unless they are found to be compatible and integral components of a building.

4. Parking Lots

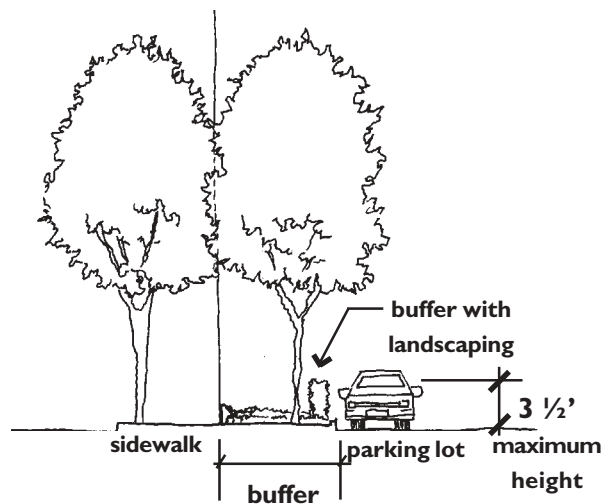
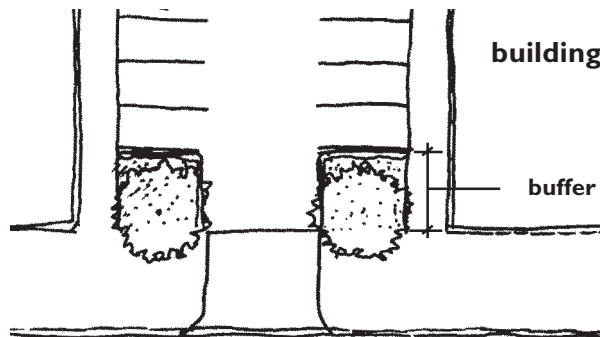
a. Location

Whenever possible, parking lots **should** be located behind a building and not adjacent to a public street.



b. Landscape Buffer

If surface parking lots face any street frontage, a landscape buffer must be provided. Plants in the buffer **should** be no higher than three-and-one-half feet in order to maintain sight distances, although occasional trees are allowed.



CHAPTER 4: DESIGN GUIDELINES



c. Landscape Aesthetics

i. Trees **should** be planted to provide shade and visual relief.

ii. The palette of plant materials in the landscape buffer **should** create an aesthetically-pleasing space through a mix of colors, heights and types of plants.



- ◆ Trees **should** provide vertical interest.
- ◆ Shrubs **should** be used to provide a natural fence that shield autos from the pedestrian sidewalk.
- ◆ Flowering plant material at the ground level is **encouraged** to provide a variety of colors.
- ◆ Creative design and the use of native plants and drought tolerant plans are **encouraged**.

d. Lighting

Lighting fixtures in parking lots **should** be selected add visual interest while complementing nearby architecture. They **should** be scaled to pedestrians.



Examples of memorable fixtures that add visual interest.

e. Parking Pavement Surfaces

i. Patterned pavement and integral colored concrete are **encouraged**, especially in parking lots that will have multiple uses or be visible to the public.

ii. Permeable surface finishes **should** be used to add surface texture and to facilitate stormwater runoff.



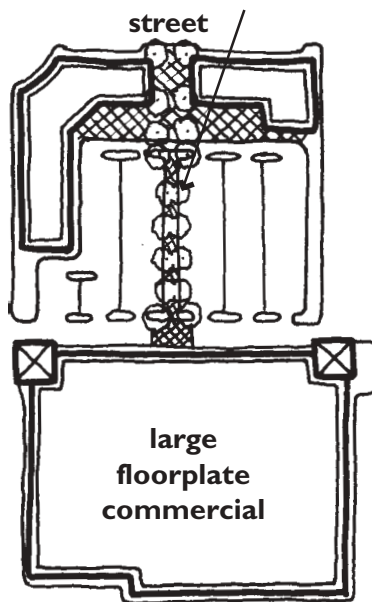
Permeable concrete paver units add texture.



f. Vertical Elements

Vertical elements such as screens or trellises **should** be incorporated into parking lots to provide visual interest, both at the parking lot edges and within the parking lot.

attractive pedestrian connection between Main Entry and parking areas

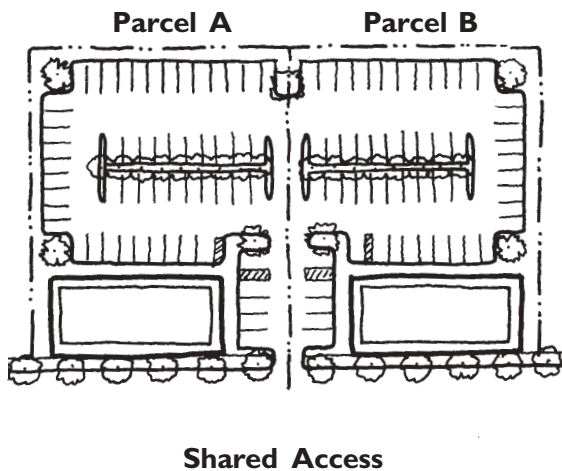


g. Pedestrian Circulation

Pedestrian walkways in parking lots **should** include elements such as special paving materials, raised curbs, trellis structures, landscaping, pedestrian-scaled lighting, seating and trash receptacles.

h. Shared Access Entries

Building siting and parking design **should** maximize opportunities for pedestrian and vehicular circulation between adjacent sites. For example, joint access ways serving two or more buildings are **encouraged** as a means to minimize curbcuts and avoid breaks in continuity of street frontages.



F. Guidelines for Primary Historic Resource Structures

The following buildings and sites are City of Fremont Primary Historic Resources in the Irvington Concept Plan area:

- ◆ Alisson Creek (Laguna Creek; ACFC parcels)
- ◆ Washington Corner early settlement
- ◆ W.W. Hirsch Building (Clark's Hall) –
4000 Bay Street
- ◆ Wright home (A.O. Rix) – 4071 Bay Street
- ◆ Odd Fellows Cemetery/Irvington Memorial Cemetery – 41004 Chapel Way
- ◆ Odd Fellows Hall –
40955 - 40961 Fremont Boulevard
- ◆ O.N. Hirsch Building –
40979 Fremont Boulevard
- ◆ Mack Grocery, Library & Post Office site –
40984 - 40986 Fremont Boulevard
- ◆ Hiram Davis home (Peixoto) –
40846 High Street
- ◆ Schoolhouse site 1875 –
Lincoln and Union Street
- ◆ Grammar School – Lincoln and Union
- ◆ Horner School Site – 3676 Union Street
- ◆ Gallegos Winery site and foundations –
3400 Washington Boulevard
- ◆ Old Corners Building site –
3996 Washington Boulevard

The City Council may designate additional buildings as Primary Historic Resources in the future. The following guidelines apply to the Primary Historic Resources in the Central Irvington area.

The guidelines for Primary Historic Resource structures are based on the "Secretary of the Interior's 1977 Standards for Rehabilitation." Included in this document is the policy statement: "...a modern addition should be readily distinguishable from the older work; however, the new work should be harmonious with the old in scale, proportion, materials, and color. Such additions should be as inconspicuous as possible from the public view."

The following is a summary of key standards:

1. The size, scale, massing and proportions of a new addition should be compatible with the historic building to ensure the historic form is not expanded or changed to an unacceptable degree.
2. The new addition should be located as inconspicuously as possible, to minimize change to the form and character of the building and to minimize loss of exterior building materials and exterior features.

3. Whenever possible, an addition to an historic building should be set back from the wall plane of the building to preserve the building's proportions and profile to the maximum extent possible.
4. Materials, color and detailing for a new addition should be compatible with the materials of the existing building, but should not attempt to replicate the materials. A visual distinction between the historic building and new addition should be made while maintaining overall design harmony for the project.
5. Additional stories should be set back from the roof edge to ensure the proportions and profile of the historic building are not radically changed.

CHAPTER 4: DESIGN GUIDELINES

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5 PLAN IMPLEMENTATION

This chapter outlines the implementation steps that the City and Redevelopment Agency plans to consider to encourage private investment and the resulting changes in the physical landscape in the Irvington Concept Plan Area. The private sector, through land development, business creation and community involvement, will play the largest role in realizing the development concepts contained within this plan. However, the City and Agency can lay the groundwork for private sector investment, and the Concept Plan can help to identify and prioritize those steps.

This Concept Plan is a long range planning document that is intended to guide the City's preparation of the documents contemplated by these implementation measures, for subsequent development over a 20-year horizon. While it is hoped that many of the concepts will be realized in early years, some concepts are dependent on public funding and/or private investments; and as such, rely on favorable economic conditions.

A. *Implementation Measures*

1. **Update Land Use and Zoning Regulations**

The Concept Plan identifies specific land use and zoning changes that would foster reinvestment and revitalization within the Irvington area. The City intends to pursue these changes as soon as possible to provide increased certainty about the City's goals and policies to the developer, tenant, property owner, and investor communi-

ties. The City intends to work cooperatively with the affected property owners to the extent possible. In the interim, individual property owners may also pursue land use and zoning amendments consistent with the concepts identified in this plan.

In addition, there are a number of related zoning tools that would facilitate the implementation of the concepts contained within this plan. These tools are discussed below:

- ◆ **Mixed Use Zoning.** An important component of the zoning plan for Irvington is the development of new mixed-use zoning regulations that stimulate reinvestment in the community. The City is presently in the process of developing a new citywide mixed-use ordinance that is intended to preserve commercial character and continuity and stimulate both development and revitalization opportunities within commercial districts.
- ◆ **Planned District Zoning.** Another useful tool that can be employed to facilitate development called for in the Concept Plan is Planned District zoning. Planned Districts can be used to craft special zoning provisions and create development incentives that respond to an area's unique characteristics and/or special circumstances. At the present time, a Planned District is being contemplated for an area along Bay Street. This particular Planned District is addressing and refining

many of the land use and zoning provisions for this area in order to achieve objectives for revitalization as an important pedestrian-oriented area within the Irvington District.

- ◆ **Design Guidelines.** Chapter 4 of this Concept Plan contains Design Guidelines that will be used to evaluate development proposals. These guidelines have been carefully developed in coordination with the community to foster the goals outlined in this plan. More refined guidelines may be adopted in the future as part of Planned Districts such as Bay Street. These guidelines would, in effect, supplement the general guidelines contained within this document.
- ◆ **Restriction of Inappropriate Uses.** Currently allowed uses may not be appropriate within new developments in some areas of the district. Automotive repair and drive-through restaurant uses are two such examples. As the City modifies the zoning for particular areas of the district it should also restrict inappropriate uses.

2. Catalyst Projects

Public improvement projects can stimulate private investment by demonstrating public commitment to an area, changing perceptions, and creating a more attractive physical environment. Public investment can also help close development feasibility gaps and support the development of "catalyst" projects that stimulate additional private development. The City and the Agency should attempt to stimulate additional privately financed development interest with

their limited abilities to fund catalyst projects. The following projects are grouped in terms of their anticipated time-frames for completion. Projects are listed alphabetically within each category.

- a. Near-term. The following projects are underway and/or are expected to be completed in the next three years:

- ◆ Fremont Boulevard Street Improvements
- ◆ Osgood Road Street Improvements
- ◆ Roberts Avenue Sidewalks
- ◆ Revitalization of the Patio World portion of the Tri-City site
- ◆ Washington Boulevard Street Improvements
- ◆ Utility Undergrounding on Bay Street, Fremont Boulevard and Washington Boulevard

- b. Mid-term. The projects are expected to be completed in between three and six years.

- ◆ Bay Street Streetscape.
- ◆ Parking lot acquisition and development.
- ◆ Bay Street Public and/or Shared Parking Facilities.
- ◆ Washington Boulevard Grade Separation.

CHAPTER 5: PLAN IMPLEMENTATION

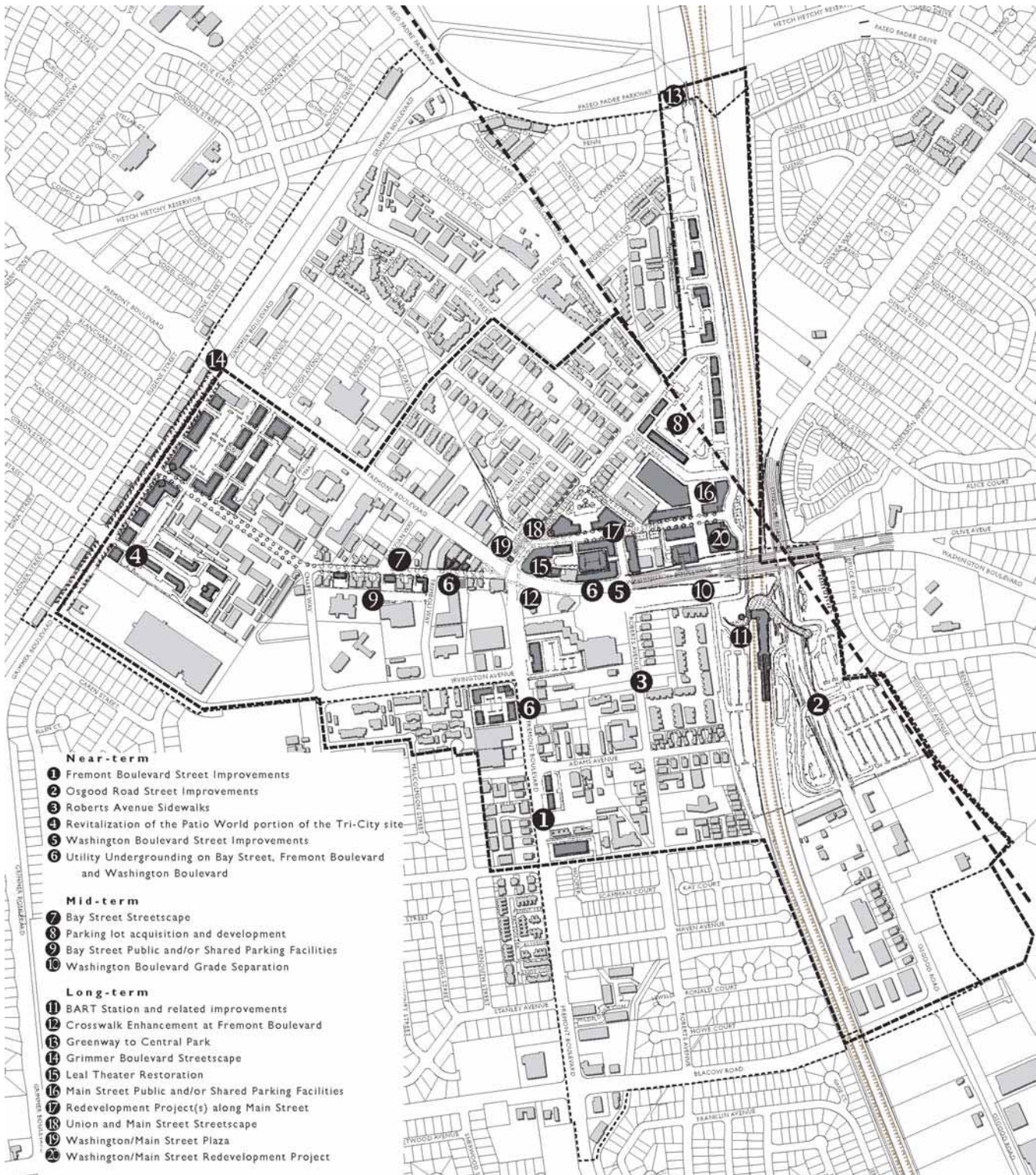


Figure 5-1 Catalyst Projects

c. Long-term. The following projects are expected to be completed in seven years or more. These projects are unfunded and are likely to require alternate funding sources, including private sector sources such as property owners, business owners and developers.

- ◆ BART Station and related circulation, access, public space and parking improvements.
- ◆ Crosswalk enhancement at Fremont Boulevard.
- ◆ Greenway to Central Park.
- ◆ Grimmer Boulevard Streetscape.
- ◆ Leal Theater Restoration.
- ◆ Main Street Public and/or Shared Parking Facilities.
- ◆ Redevelopment Project(s) along Main Street.
- ◆ Union and Main Street Streetscape.
- ◆ Washington/Main Street Plaza.
- ◆ Washington/Main Street Redevelopment Project.

3. Other Public Actions

The City and the Agency can bring resources to bear on the Irvington district in addition to funds and capital projects. These resources include specialized expertise, industry relationships, and facilitation. The City should dedicate these resources to establish organizational structures and focused efforts such as:

- ◆ **Business/Property Owner Improvement District for Bay Street.** The City can work with property owners and merchants to establish a Business or Property Owner Improvement District (BID or P-BID) for Bay Street and possibly Main Street in the future. P-BIDs require owners of more than 50 percent of the property to vote to establish an annual assessment on parcels. The funds generated from this assessment can provide higher levels of maintenance and security, marketing, and other services. A P-BID can help promote the identity of Bay Street and cause a higher level of property owner involvement. Bay Street is a good candidate for a BID or P-BID because the Bay Street property owners are relatively organized, the area is distinct from other Concept Plan areas and the street and property owners could capture most of the benefit from the services provided through the BID/P-BID.

- ◆ **Parking District for Bay Street.** The establishment of a "district" approach maximizes shared use of parking areas by allowing businesses and new developments to use shared offsite parking to fulfill parking requirements. Such an approach can enhance the feasibility of developments because a shared arrangement is often more efficient and cheaper than on-site parking. The parking district may also introduce in-lieu fees for on-site parking requirements or even paid parking. In addition, because a parking field is created, the City could consider reducing the parking requirements to reflect the efficiency of the shared arrangements. Bay Street is the best candidate for this strategy because small lot sizes and the planned Streetscape Improvements project constrain on-site parking. Main Street is a future candidate for a parking district.
- ◆ **Land Assembly.** The small size of parcels in a number of the key site locations is an impediment to private development. Working with multiple owners to assemble sufficiently large sites is extremely difficult, time consuming, and often fails due to lack of cooperation by one or more property owners. The Redevelopment Agency can play a key role in assembling parcels at key sites through voluntary purchases or, if necessary, use of its eminent domain authority.
- ◆ **Marketing and Tenant Improvement.** The Redevelopment Agency and the City can collaborate with property owners to undertake a marketing campaign to highlight the unique ambiance of the district, demographic information, the City's investment in the area and possibly a new regulatory climate that will provide a greater level of certainty about development opportunities and the community's expectations for those opportunities. In addition, the City and the Agency can assist private developers and property owners with tenant recruitment, either through direct contact with potential retailers or through sponsorship of events such as the developer/retail expert panel held as part of the Irvington Concept Plan process. The City or the Agency should package resources and information based on a business attraction strategy that synthesizes the Irvington and city-wide retail studies recently undertaken. The City should also consolidate zoning and other entitlement information in a document that developers can easily understand.

- ◆ **Strengthening Business/Community Capacity.** The City is working to support and strengthen the skills and abilities of small business by offering opportunities to succeed in today's market. For example, the City, in collaboration with business trade organizations, offers on-going small business workshops that address the development of successful and creative marketing campaigns to foster increased patronage and sales. Additionally, the City is revising its Commercial Rehabilitation Loan Program to allow flexibility for a separate and distinct Façade Improvement Grants Program. The proposed program aims to improve the appearance of storefronts in the Centerville, Irvington, and Niles project areas making shopping more attractive to both shoppers and retailers. The Commercial Rehabilitation Loan Program will continue to provide short term loans to assist property owners reconstruct or rehabilitate their property to address seismic retrofitting, code compliance, and new use improvements in the three Redevelopment project areas.

The City also plays a role in strengthening the capacity of communities by encouraging residents to transform their neighborhood through active participation in the planning and design process. The City also networks with local business organizations, such as the Irvington Business Association (IBA), and supports their efforts to improve the Irvington business environment.

B. Phasing of Implementation Measures

The numerous capital projects planned for Irvington over the next several years are the foundation of the Concept Plan and represent the first phase and highest priority for implementation of the Concept Plan. These projects are all in the planning phase now, with some entering the construction phase.

Next to completing the planned capital projects, the highest priority project for the City should be to implement the land use and zoning changes that are needed in order for the Concept Plan to be realized. The City should initiate a re-zoning effort in concert with affected property owners. In general, the contemplated re-zoning should add flexibility to future land use and increase property values. The re-zoning efforts can be refined to respond to as many neighborhood concerns as possible.

To the extent that additional resources become available for implementation of the Concept Plan, these resources should be focused to leverage other investments in the area. The logical area on which to focus additional resources is the Five Corners area. As the central intersection and historic core, this area more than any other defines the district and the impact of improvements to the area should radiate outward to the other focus areas identified in the Concept Plan. Through the Bay Street streetscape improvements, the Agency is making a significant investment to enhance public and private spaces. If

additional investments, such as public parking facilities or other Agency-assisted land development, can be made to leverage these improvements, the payoff could be an attractive, thriving Five Corners that makes private sector investment more attractive elsewhere in the district.

Clearly the most important variable in the implementation of the Concept Plan is whether the City is able to amend its Redevelopment Plan to facilitate the construction of the Irvington BART station and related improvements. These improvements are not funded. With the appropriate related improvements, such as well-designed pedestrian and vehicular access from Main Street and Roberts Avenue and public spaces, the BART station should induce considerable private investment in the area. At this point, however, the BART-related improvements are long-term priorities for implementation of the Concept Plan, although planning for these improvements and securing funding through the Redevelopment Plan Amendment process are top priorities. Even without a near-term BART Station, the modifications to Washington Boulevard and the railroad track realignments required for the grade separation project, in combination with regulatory changes, may encourage significant new private investment along Main Street and in its vicinity.

The implementation matrix on the following pages provides:

- ◆ A summary of recommended implementation steps for the Concept Plan
- ◆ A general time frame for the completion of implementation steps
- ◆ A rough cost estimate
- ◆ The department/division responsible for leading the project

This information is very preliminary. The information will change as opportunities arise to implement projects. The cost estimates are not engineering estimates; they are placeholder figures that provide an order of magnitude for the costs associated with projects. To describe the suggested timeframe for projects, near-term refers to projects that should be underway and nearing completion in the next three years. Mid-term refers to projects that are three to six years out, and long-term refers to projects that are more than six years out.

The implementation matrix is intended to serve as a work plan for City and Agency staff. Staff would provide the City Council with annual updates regarding progress on implementing the Concept Plan.

CHAPTER 5: PLAN IMPLEMENTATION

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Implementation Matrix for the Irvington Concept Plan

Description	Primary Justification (Concept Plan Goal)	Timeframe	Estimated Cost	Project lead	Notes
Districtwide					
<u>Business Attraction Strategy</u> . Recommendations for retail and business attraction developed from assessment of district market conditions. Marketing and resource materials developed for prospective developers, tenants and brokers.	Attract unique shopping, dining and neighborhood services to Irvington	Near-term	\$20,000	Econ Dev. OHR	Much of this work has been completed. Retail attraction strategy developed to be in conjunction with a major redevelopment project.
<u>Mixed Use Zoning</u> . Modifications to Mixed-use zoning designation to encourage mixed use development by allowing higher density development and significant residential components, and to define parking standards and live-work space.	Encourage revitalization of underutilized parcels.	Near-term	\$75,000	Planning	Current zoning requires 51% of new space to be commercial and does not allow the development densities contemplated in Concept Plan. City-wide Update in process.
<u>Rezoning</u> . Implement General Plan and Zoning Map changes to comport with Concept Plan with cooperation of affected property owners.	Encourage revitalization of underutilized parcels.	Near-term	\$75,000	Planning	To follow modifications to zoning to encourage mixed use development. Some rezoning to occur through the Planned District approach, which would proceed on a separate track.
Bay Street					
<u>Monument Center Renewal Project</u> . New retail, parking, housing and public space developed to revitalize Bay Street and Five Corners area.	Attract unique shopping, dining and neighborhood services and create distinctive identity.	Near-term	N/A	Private	Facilitate catalyst development. Should be able to occur with no public subsidy (unless for affordable housing). This site led by private sector
<u>Utility Undergrounding</u> . Telephone, electrical and cable distribution lines placed underground behind sidewalk.	Improve overall appearance of district.	Near-term	N/A	Engineering	Use of Rule20A funds approved for this project. Planning to begin in 2004-2005.
<u>Streetscape Project</u> . Schematic design in Concept Plan (Figures 3-5A and 3-5B) refined and constructed.	Improve streetscape design and create distinctive identity	Near-term	3.5 million	Engineering	Parking arrangements should be developed in conjunction with streetscape.
<u>Public and/or Shared Parking Facility</u> . Parking to induce investment along Bay Street properties developed. Parking could be public or serve as shared private parking for Bay Street businesses.	Develop parking to meet commercial needs.	1-5 years	TBD	OHR	
<u>Business Improvement District</u> . Or Property Owner Business Improvement District. Funding mechanism established for projects such as marketing, security, and maintenance.	Improve overall appearance of district.	Near-term	TBD	Econ Dev OHR	Monument Center will be key property owner on street.
<u>Parking District</u> . Shared parking arrangements, including cost-sharing, instituted among Bay Street property owners.	Develop parking to meet commercial needs.	Near-term	TBD	OHR	Should be developed in conjunction with Bay Street.
<u>Planned District</u> . Specific land use regulations adopted to implement a high-quality development.	Improve overall appearance of district and create distinctive identity	Near-term	TBD	Planning	To be developed in concert with creation of a BID and Parking District. To follow creation of Mixed Use Zoning.
<u>Crosswalk Enhancement at Fremont Boulevard</u> . Current crosswalk connecting Bay Street to Union Street improved.	Provide safe and well-designed pedestrian network and minimize impact of through-traffic	Mid-term	TBD	Engineering	
<u>Commercial Rehabilitation and Façade Improvement Program</u> . Property owners utilize loans and grants for property improvement.	Encourage revitalization of underutilized parcels.	Near-term	TBD	OHR	Should be discussed with property owners during Streetscape development and related right-of-way acquisition.
<u>Infill Development / Rehabilitation</u> . New development and investment undertaken consistent with Concept Plan.	Revitalize underutilized parcels.	Near-term	TBD	Private	Should be able to occur with minimal public subsidy (by way of OPAs).
Washington / Main Street Triangle					
<u>Landscaping / Plaza</u> . Conceptual design for a plaza (see Figures 3-8 and 3-10) refined and constructed.	Improve overall appearance of district and pedestrian network.	Mid-term	TBD	Engineering	
<u>Lea Theater</u> . Rehabilitation and continued reuse of historic theater.	Develop distinctive identity and encourage revitalization of historic buildings.	Mid-term	TBD	Private	Will require public assistance by way of historic preservation grant or RDA Commercial rehab loan program

Implementation Matrix for the Irvington Concept Plan

Description	Primary Justification (Concept Plan Goal)	Timeframe	Estimated Cost	Project lead	Notes
<u>Land Assembly</u> . Facilitate developer interest to assemble a developable property.	Encourage revitalization of underutilized parcels.	Mid-term	TBD		Current parcel configuration not suitable for accomplishing Concept Plan vision.
<u>New Development</u> : Identify a developer for undertaking buildings.					
<u>Planned District</u> . Specific land use regulations adopted to implement a high-quality development.	Improve overall appearance of district and create distinctive identity	Near-term	TBD	Planning	Also prevents inappropriate development, which could occur with current zoning.
<u>Infill Development</u> . New development, including onsite parking, undertaken consistent with Concept Plan.	Encourage revitalization of underutilized parcels.	Mid-term	TBD	Private	Likely to be result of Agency-facilitation efforts.
Main Street					
<u>Streetscape Project</u> . Conceptual design in Concept Plan (Figures 3-11 and 3-13) refined and constructed. Includes modification to Main & Lincoln intersection and traffic calming devices.	Improve streetscape design and pedestrian network.	Long-term	TBD	Engineering	Contingent on RDA Plan Amendment.
<u>Public and/or Shared Parking Facility</u> . Parking to induce investment along Main Street properties developed. Parking could be public or serve as shared private parking for Main Street businesses.	Develop parking facilities to meet commercial needs.	Long-term	TBD	OHR	Contingent on RDA Plan Amendment.
<u>Land Assembly (Grade Separation properties)</u> . Respond to property owner / developer interest to assemble a developable parcel on multiple properties adjacent to and including the properties impacted by Washington Blvd Grade Separation.	Encourage revitalization of underutilized parcels and integrate BART into neighborhood.	Mid-term	TBD	OHR	Contingent on RDA Plan Amendment.
<u>Land Assembly (Other)</u> . Respond to property owner / developer interest to assemble a developable parcel.	Encourage revitalization of underutilized parcels.	Long-term	TBD	OHR	Contingent on RDA Plan Amendment.
<u>Developer Solicitation</u> . Working with property owners to identify a developer for undertaking buildings consistent with Concept Plan.	Attract unique shopping, dining and neighborhood services and create distinctive identity.	Mid-term	TBD	OHR	
<u>Owner Participation Agreements</u> . Specific agreements reached with select property owners regarding improvement to property.	Encourage revitalization of underutilized parcels.	Long-term	TBD	OHR	Contingent on RDA Plan Amendment.
<u>Infill Development</u> . New development undertaken consistent with Concept Plan.	Encourage revitalization of underutilized parcels.	Long-term	TBD	Private	Likely will result from RDA-lead land assembly, developer solicitation, and OPAs.
BART					
<u>Washington Boulevard Grade Separation</u> . Overpass designed and constructed for Washington Boulevard to go over railroad and future BART tracks.	Minimize impact of through-traffic while meeting circulation needs and improve pedestrian network	Mid-term	\$70,000,000	Engineering	Design underway. Construction to start in 2004-05. Cost of Paseo Padre grade separation included.
<u>Redevelopment Plan Amendment</u> . Amend Industrial Redevelopment Plan to provide funding for Irvington BART Station and related improvements.	Improve all transportation networks and integrate BART Station into district.	Near-term	\$700,000	OHR	Staff authorized to begin amendment process.
<u>BART Station</u> . Property acquired and platforms, parking and station structure designed and constructed.	Improve all transportation networks	Mid-term	\$73,000,000	BART	Contingent on RDA Plan Amendment.
<u>Planned District/General Plan Amendment</u> . Specific land use regulations adopted to comport with public facility use.	Improve overall appearance of district and create distinctive identity	Mid-term	TBD	Planning	BART should initiate process.
<u>Treatment of Main Street Terminus</u> . Attractive screening, pedestrian circulation, and open space designed and constructed to shield Main Street area from BART tracks. High Street modified and roadway constructed between High Street and grade separation.	Integrate potential future BART station and improve overall appearance of district.	Mid-term	TBD	Engineering	Contingent on RDA Plan Amendment.

Implementation Matrix for the Irvington Concept Plan

Description	Primary Justification (Concept Plan Goal)	Timeframe	Estimated Cost	Project lead	Notes
<u>Multimodal Access to Main Street.</u> Access routes with high quality, pedestrian oriented amenities (i.e., lighting, landscaping) designed and constructed to connect BART station to Main Street under the grade separation.	Integrate potential future BART station and improve pedestrian, bike and bus network	Mid-term	TBD	Engineering	Contingent on RDA Plan Amendment.
<u>Multimodal Access to Roberts Avenue.</u> Access routes with high quality, pedestrian oriented amenities (i.e., lighting, landscaping, trash receptacles) designed and constructed to connect BART station to Robert Avenue.	Integrate potential future BART station and improve pedestrian, bike and bus network	Mid-term	TBD	Engineering	Contingent on RDA Plan Amendment.
<u>Public Plaza at Station Entrance.</u> Standard Bart Station enhanced with plaza features such as benches and public art.	Integrate potential future BART station and develop distinctive identity	Mid-term	TBD	Engineering	Contingent on RDA Plan Amendment.
<u>Design and Materials Enhancement of Station.</u> Standard Bart Station architecture enhanced to produce a high quality, distinctive neighborhood station.	Integrate potential future BART station and develop distinctive identity	Mid-term	TBD	Engineering	Contingent on RDA Plan Amendment.
<u>Infill Development along New Street.</u> New development undertaken consistent with Concept Plan.	Encourage revitalization of underutilized parcels.	Long-term	TBD	Private	Should be able to occur with minimal public subsidy.
Railroad Parcels					
<u>Railroad Realignment.</u> Existing Union Pacific tracks realigned and current right of way abandoned.	Encourage revitalization of underutilized parcels.	Near-term	N/A	Engineering	Included in scope of grade separation project.
<u>Planned District/General Plan Amendment.</u> Properties rezoned to higher density residential with greenway provided owner(s) makes improvements to land consistent with Concept Plan.	Encourage revitalization of underutilized parcels.	Mid-term	TBD	Planning	Pursue when BART and other infrastructure plans are final.
<u>Greenway to Central Park.</u> A path with open space and pedestrian amenities (i.e., lighting, benches, trash receptacles) from the Irvington BART Station to Central Park scoped out and constructed. A pedestrian/bicycle bridge built over Paseo Padre.	Improve pedestrian and bike network	Long-term	TBD		
<u>New Street.</u> A new residential street to provide access to the railroad properties to be engineered and constructed.	Integrate BART station and accompanying residential development into district.	Long-term	TBD		
<u>Infill Development.</u> New development undertaken consistent with Concept Plan.	Encourage revitalization of underutilized parcels.	Long-term	TBD	Private	Should be able to occur with no public subsidy (unless for affordable housing).
Fremont Boulevard					
<u>Utility Undergrounding.</u> Telephone, electrical and cable distribution lines placed underground behind sidewalk.	Improve overall appearance of district.	Near-term	N/A	Engineering	Project on schedule to be complete by end of year 2004.
<u>Streetscape Project.</u> Street widening, sidewalks, and landscaping designed and constructed.	Improve streetscape design and pedestrian network.	Near-term	\$4,200,000	Engineering	Project on schedule to be complete by end of year 2004.
<u>Land Assembly.</u> Multiple properties assembled into a developable property by a developer or the Agency.	Encourage revitalization of underutilized parcels.	Mid-term	TBD	OHR	Current parcel configuration not suitable for accomplishing Concept Plan vision.
<u>Developer Solicitation.</u> Working with property owners to identify a developer for undertaking buildings consistent with Concept Plan.	Encourage revitalization of underutilized parcels.	Mid-term	TBD	OHR	
<u>Owner Participation Agreements.</u> Specific agreements reached with select property owners regarding improvement to property.	Encourage revitalization of underutilized parcels.	Mid-term	TBD	OHR	Contingent on RDA Plan Amendment.
<u>Infill Development.</u> New development undertaken consistent with Concept Plan.	Encourage revitalization of underutilized parcels.	Mid-term	TBD	Private	Should be able to occur with minimal public subsidy (by way of OPAs or Affordable housing sources).

Grimmer South

Implementation Matrix for the Irvington Concept Plan

Description	Primary Justification (Concept Plan Goal)	Timeframe	Estimated Cost	Project lead	Notes
<u>Streetscape Project</u> . Street improvements and landscaping designed and constructed.	Improve streetscape design and pedestrian network.	Long-term	TBD	Engineering	
<u>Developer Solicitation</u> . Working with property owners to identify a developer for undertaking buildings consistent with Concept Plan.	Encourage revitalization of underutilized parcels.	Long-term	TBD		
<u>Mixed Use Zoning</u> . Modifications to Mixed-use zoning designation to encourage mixed use development by allowing higher density development and significant residential components, and to define parking standards and live-work space.	Encourage revitalization of underutilized parcels.	Near-term	\$75,000	Planning OHR	Current zoning requires 51% of new space to be commercial and does not allow the development densities contemplated in Concept Plan. City-wide Update in process.
<u>Infill Development</u> . New development undertaken consistent with Concept Plan.	Encourage revitalization of underutilized parcels.	Long-term	TBD	Private	Size of sites should attract private sector investment.
Creeks					
<u>Laguna Creek Watershed Restoration</u> . Restoration plan implemented to extent possible.	Improve access to Laguna creek.	Long-term	TBD	Env. Svcs.	Restoration Plan adopted in July 2000. Potential project in conjunction with Washington/Main Triangle improvements or with infill development between Fremont and Roberts.